

Skatepark Business Plan

Overview

Mumbles Community Council plans to build a Skatepark located on Mumbles Promenade at the location of the current half-pipe ramp. This is the preferred location as confirmed by the Independent Consultant Report (see Annexe 1)

Mumbles Community Council have contracted Maverick who are a world class Skatepark build and design company having built over 100 Skateparks in the UK including the iconic Concrete Waves Skatepark in Newquay and locally in Penarth, Haverford West, Aberdare, Mountain Ash and Porthcawl. As contractors they Project Manage all aspects of the design, development, build and maintenance of the site from pre-planning through to on-going inspections and maintenance once the build is complete.

Maverick have incorporated ideas from local school children, BXM riders from the UK Olympic Team and the World Champion of wheelchair motocross into the design and while this may have cost more than other designs, MCC recognised the importance of creating a world class design to complement the iconic location and one that provided full and safe access to all.

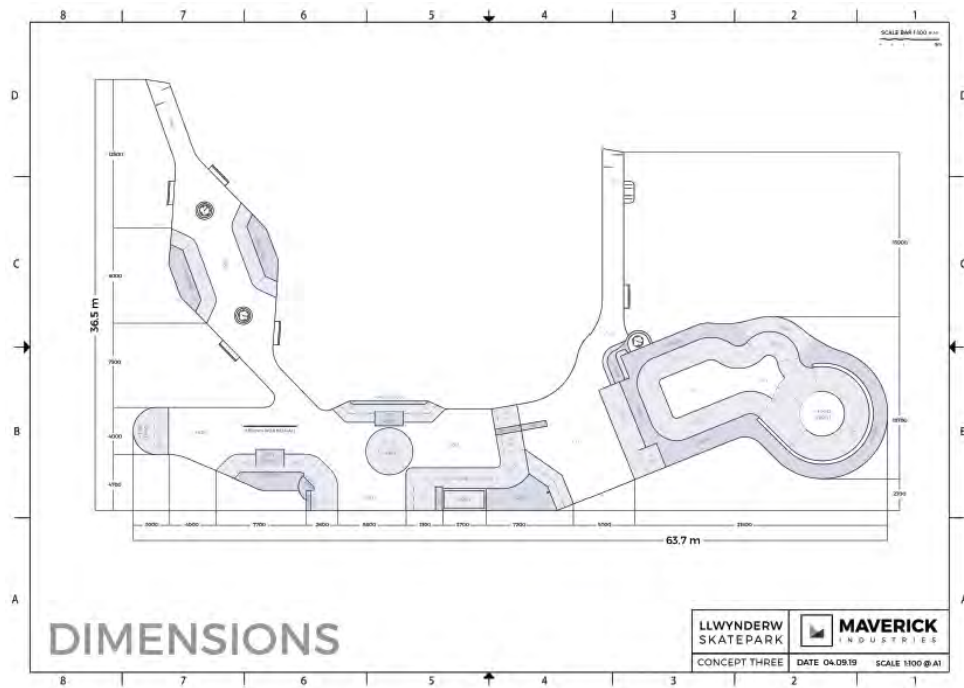
Part 1

The Building

Maverick have undertaken comprehensive surveys of the site including Geotechnical, Topographical, Trees, Ecology, Flood Risk, SUDS to ensure that the site is suitable for the planned design and build.

The Skatepark will roughly follow the footprint of the ramp and adjoining tarmacked area to the West meaning it will be primarily set below the sightline from Mumbles Road.

The actual build site required is 748 sq. m being 63.7m by 36.5m at its widest point.



The site will be landscaped with grass and wildflowers and will be protected from the road by a **hawthorn hedge fronting a 1.2m fence extending the full length of the site.**

The skatepark itself will be constructed from free form spray concrete with - in certain areas - narrow (25mm box section) galvanised steel edging to prevent chipping and it will be finished in a neutral and light grey colour but with flashes of colour for aesthetic purposes.

The site will be enhanced by a drinking fountain, picnic table seating, and footpaths with wheelchair accessible chicane entrances on the promenade entrances.

(see Annexe 2: **Mumbles Design Access Statement** for full details)

Realisation

Prior to any building works the contractor will create a secure site compound with herras fencing and temporary trackways for site access. Full details of the gradient and minimizing sideways slope are included in Annexe 3: **Pre Construction Phase Health and Safety.**



All traffic movements for the site and building works will comply with the Traffic Management Plan as approved by Swansea Council Highways Department. This sets out restrictions on delivery timings to avoid busy times such as school runs and rush hours.

All required safety signage will be put up on the approaches to the site on Mumbles Road and on site and all required contractor parking will be on site and will have no impact on nearby streets. All traffic movements will be controlled by Maverick's own certified banksmen.

A warning sign for pedestrians will be placed at the entrance of both our site and the access road. Additional warning signs will be placed on the perimeter fence. Site crew will informally direct the public away from site if appropriate. CCTV cameras will monitor the site overnight and intruders will hear an oral warning. One of Maverick's Directors will also be alerted by the system.

For full details of the Health and Safety Plan to be implemented see Annexe 3.

The main elements of the build will be:

- site preparation which includes, creating an overall site compound with herras fencing and the laying of a temporary trackway system
- • Decommissioning the old redundant skate ramp
- • Excavating some soil in the bank and building up with compacted stone in other areas, forming the shapes needed to create the design
- • Forming a steel grid system over the shapes to comply with quality standards

- Applying a 150mm deep layer of concrete in manageably small bays over the entire structure.
- Formation of concrete pathways and installation of fencing, ancillary items & planting as indicated in the supplied drawings

In order to ensure that construction complies with all necessary European and National Standards as well as Building Regulations, a detailed and Comprehensive Concrete Construction Phase Plan has been produced (see Annexe 4: **Concrete Construction Statement**). This Statement covers all aspects of construction using concrete including; materials specifications, environmental health and safety and personnel health and safety with particular focus on minimizing and protecting from dust. Mumbles Community Council will oblige Maverick to comply with all recommendations contained within this statement at all times.

Contract Management

Maverick are contracted to provide comprehensive Project Management of all aspects of the design, development and completion of the Skatepark and associated landscaping. Maverick are highly experienced having project managed the design and build of over 100 Skateparks in the UK. MCC will also appoint a Consultant who will assist in the development of a Project Plan for the Skatepark in conjunction with Maverick Industries and will oversee all aspects of the development on behalf on MCC and agree the final sign-off (See Annexe 5: **Management Procedures Statement**)

Maverick have full insurance cover for all aspects of the project including Public Liability Insurance.

Costings

Pre Construction Phase

User Consultation Post Tender to create final design included free of charge	£0.00
Support with documentation required for Planning Application (excludes surveys)	£0.00
JCT Contract Documentation (MW 2016 with Contractor's Design) supplied & completed free of charge	£0.00

Detailed Design

Engineers Site Visit, Topo Survey, Set Stations, CAT Scan & Utilities checks (completed)	£1,500.00
Drainage Survey - Trial pits & Percolation Tests (completed)	£2,750.00
H&S and Project Planning	£5,000.00
Detailed Design Engineering - Suite of Drawings for Construction	£30,000.00

Construction Phase

Preliminaries

Insurances. Heras, signage, welfare & store, plant hire, transport & fuel, skips	£53,200.00
CCTV 24hr security monitoring of site (12 weeks)	£6,000.00

Groundworks Phase

Temporary trackway	£5,200.00
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Remove/recycle miniramp	£750.00
Set out to Engineers Stations	£3,000.00
Installation of drainage as per engineering design	£6,800.00
Remove tarmac where required, foundations, block walls, rebar & concrete infill	£14,100.00
Importation & compaction of stone	£44,300.00
Installation of pool copings	£3,400.00
Steelworks Phase	
Off site steelworks, galvanising, fabrication & rolling	£14,300.00
On site steelworks, installation of copings & framework system	£27,900.00
Shuttering, reinforcing & tying	£13,200.00
Concrete Phase	
Pour/Spray and finish, concrete platforms, transitions & floors incorporating colouring as per visuals	£67,300.00
Perimeter bunding & landscaping	
Creation of bunding using excavated material, topsoil and seed.	£8,200.00
Installation of art deco steps/planter	£7,200.00
Planting of 3 Palm Trees c. 7-8ft	£1,500.00
Planting - 90m of 3ft high Hawthorn - species to be agreed	£2,400.00
Ancillary works	
Supply and installation 2no Wheelchair access Picnic Benches	£1,620.00
Supply and installation 3no Glasdon Chieftain Litter Bins (or similar per Council preference) on concrete plinth	£1,425.00
Supply and install 90 linear metres of green bow top fencing 1.2m high	£8,000.00
Supply and install 2no Galvanised Steel Chicane entrances	£1,500.00
Supply and install 5no Benches	£2,500.00
Supply and install 2no galvanised steel cycle racks	£600.00
Ancillary works	
Supply & install of 1no RoSPA compliant skatepark A1 skatepark safety sign on post	£350.00
Railway sign	£600.00
RoSPA PI Inspection on handover	£395.00
Council launch event (See event package)	£2,000.00
Contingency	
Access/Traffic Management Contingency sum subject to planning authority requirements - PC Sum	£5,000.00
TOTAL: (EXCLUDING VAT)	£334,990.00
Water Fountain supply and installation, including laying water supply	£7,500
Additional Benches and Bins	£2,500

Funding

Build

Mumbles Community Council has approved underwriting the full costs of the build. (See Annexe 6: **Council Minutes June 9th 2020** and Annexe 6a **Mumbles Skatepark Funding Proposal**). In order to minimise costs and allow for additional projects to be funded, MCC

will apply for grant funding through the National Lottery People and Places Fund. If successful this will reduce the Council's funding commitment by £232,240.

Ongoing maintenance costs

Mumbles Community Council has already committed to funding up to £12,698 per annum (see Annexe 6a) for annual maintenance and the provision of an annual £6,700 sinking fund representing 2% of build costs for major refurbishment/rebuild (expected minimum 25 years life). See below for income generation plan to minimise MCC's annual commitment. Any shortfall in income to cover the full cost of £13,945.67 (£19,945.67 in Yr1) will be met by the annual budget of the Community and Social Well-being Committee, or the Council contingency fund.

Risks

SWOT Analysis

Strengths	Weaknesses
<ul style="list-style-type: none"> • Planning permission gained • Uses brownfield site • Proven success of existing site • Strong local support, including from local PCSOs, for new site • Involvement of public through meetings and surveys • Enthusiastic team • Strong youth involvement • Low future overheads • Positive engagement with SCC • Working with experienced project managers • Build cost fund in place • Parking available at Blackpill • Enhances existing activity hub • Encourages active travel with smooth paths and bus routes • Shops and cafes close by in West Cross shopping centre and Blackpill • Public Toilets at Blackpill • Good visibility for safety and security 	<ul style="list-style-type: none"> • Potential parking issues* • First experience of build for MCC • Potential noise increase for nearby residents • Safety barrier (green hedging) may alter current visual aspect •
Opportunities	Threats
<ul style="list-style-type: none"> • Positive way to engage local youth • Great national exposure for the area • Facilities for disabled and all ages and abilities • Marketing opportunities 	<ul style="list-style-type: none"> • Small faction of local opposition • Exposed to potential vandalism • Potential target for drug dealers • Covid-19 hampering progress • Possible safety issues with a busy road

<ul style="list-style-type: none"> • Benefit to the wider Swansea community • Local and National competitions • Potential to cover running costs through sponsors • Communication through social media • Fundraising opportunities • Tourism opportunities • Park and Ride potential from St Helens* 	
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See also Annexe 1

Risk / Hazard / Action Matrix

RISK	PEOPLE AFFECTED	EXISTING CONTROL MEASURES	FURTHER PREVENTATIVE ACTION REQUIRED
Collision with vehicles not related to construction	All those with vehicles within our sphere of operations	Existing speed limits Warning signs	Issue instructions to all delivery companies and staff that crawling speeds are mandatory with hazard lights turned on. Use of banksmen to guide delivery vehicles
Collision with children or visitors beside the site	Any visitor to the fields beside the trackway and crew operatives driving to site	Relatively few people are likely to be around us when any traffic movements are taking place There is no pavement on the side of the road we enter on so this will minimise the risk of meeting pedestrians walking beside where we would enter	Placement of construction signs warning of works close to the site entrance Approaching the construction site entrance drivers to switch on hazard lights When on the haul road drivers will reduce speed to a crawl and only move with the aid of a banksman
Delivery Vehicles getting stuck on site	All delivery vehicles and site crew vehicles	The approach to the site is mumbles road, immediately this is left, drivers will be on a temporary steel trackway	Use a banksman in and out of site and ensure drivers stay on the haul road Request delivery drivers to turn around when in site and drive back out of the site on the haul road facing forward
Delivery vehicles cannot gain entry due to parked cars	All delivery vehicles and site crews	While there are NO no parking signs beside the site ,or road markings prohibiting parking, cars do NOT park on the road here	No further action required other than placing a sign on the heras fencing at the entrance to the haul road

		because of lack of space to do so.	stating not to park in front of the gates
Delivery vehicles damaging buried services	The owners of the buried services	The site has had services checks carried out and all known services have been identified and the invert levels are known. The site has also been CAT scanned to verify the positioning of services, and they match the records received with no others on site identified that were otherwise unknown	To further protect the services Aluminium track way will be laid over these service runs (only in one position)
Keeping the highway free of site generated mud and debris	All road users	A temporary aluminium trackway will be laid to ensure delivery and egress routes will remain clean	A water bowser and pressure washer will be on site to clean any delivery vehicle wheels if it is needed to be don to protect the highway.
Control of traffic while delivery vehicles are exiting the site	All traffic using Mumbles road at the time of egress from site Of any vehicle	A qualified Banksman wearing PPE will be controlling entry and egress of delivery vehicles Copies of training certificates have been provided to Dean Howard Swansea Council's Street works and Winter Services Manager	Banksmen will utilise chapter 8 STOP GO boards to control motorists on Mumbles road while the vehicles are exiting the site
Contractor vehicle parking and material storage – ensuring there is no disruption to local residents	All members of Maverick and local residents	Crews will share vehicles to get to site Crews are not permitted to park on the road	No further control measures needed. There is adequate carparking within the site for all our crew and for material storage, materials will be unloaded within the site also.
Emergency Contact details	Anybody with the need to make emergency contact with the contractor	Adequate signage giving emergency contact numbers will be displayed in various positions around the site attached to the heras fence line	The design and content of this board will be agreed with the client but an example board has been provided within this document
Adequate insurance for works on site is needed , specifically Liability Insurance for £10million		A copy of this insurance have been provided to Dean Howard- Swansea Council's Street Works and Winter services Manager	
Damage to the highway or kerb stones at entry / exit point	Users of the highway	A photographic condition survey will be taken by maverick prior to the commencement of works of the approach to and the entry point to the site	Damage is not expected to take place however the site crew will be instructed to monitor the condition of the approaches and should damage become evident they call contact the office

			to seek advice on what they should do to remedy it
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Build Timeline

The build is expected to take approximately 16 weeks. Exact timescale and build timeline will be completed once the start date is agreed as timings will depend on the time of year when the build is to be completed. Optimum time periods are during Spring or Autumn but work can take place at any time throughout the year.

Area of Land Needed

The preferred site for the Skatepark is on part of the area of land identified by Swansea Council in the PIN notices as Land around the Skatepark, West Cross. The build will require a total of 748 sq. m. It will run from, and include, the land currently occupied by the Skate ramp and will extend West to include the area that is currently tarmacked. An area of land surrounding the Skatepark will be used for picnic tables and benches, a bike stand, water fountain and litter bins. The whole area will be landscaped using a wildflower grass mix that has been specially mixed to suit the Gower landscape and climate.

MCC has costed in for landscaping and maintenance of the whole PIN area, which is 2.5 acres, as identified on the map below, and will take responsibility for the whole area if that is Swansea Council's preferred choice.



What will it look like?

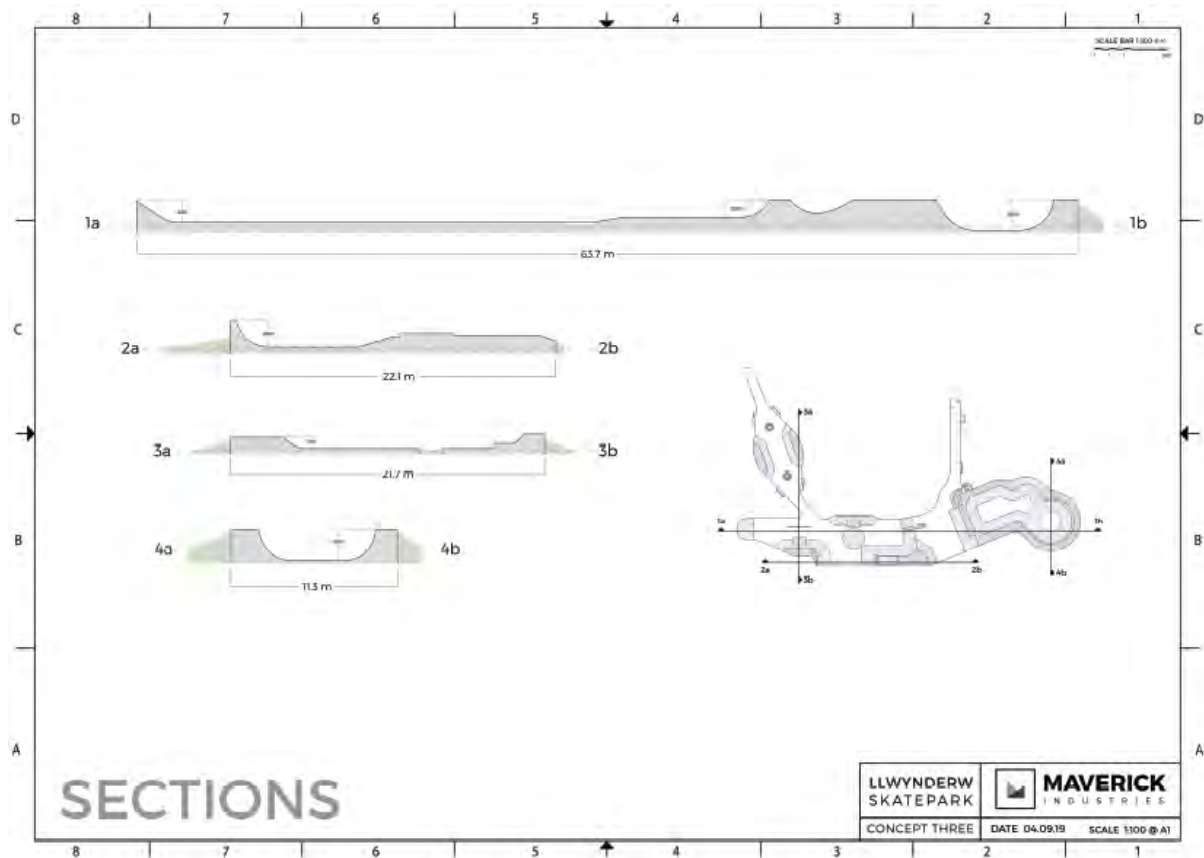
The site will be landscaped with a meadow grass wildflower mix (Gower Mix) and will be protected from the road by a **hawthorn hedge fronting a 1.2m fence extending the full length of the site**. The Skatepark itself will be sympathetically banded allowing grass to grow up to the edge of the platforms and designed to take advantage of the natural topography which will further disguise its existence to other park users and members of the public not interested in skating.

The skatepark itself will be constructed from free form spray concrete with - in certain areas - narrow (25mm box section) galvanised steel edging to prevent chipping and it will be finished in a neutral and light grey colour but with flashes of colour for aesthetic purposes.

The site will be enhanced by a drinking fountain, picnic table seating, and footpaths with wheelchair accessible chicane entrances on the promenade entrances.







Design Development

The final design for the Skatepark has been developed over 18 months to ensure that all interested parties - residents, Skateboard groups, all local schools – had an opportunity to input into it through public meetings and consultations and questionnaires and surveys. We also were able to incorporate BMX facilities taking advice from a member of the UK Olympic BMX team and full disability access with advice from the Wheelchair Motocross World Champion. This has inevitably added to the cost but MCC were committed to ensuring that the Skatepark could be used by all ages and abilities as well as providing a world class facility in keeping with the iconic location. The siting has allowed for most of the Skatepark to be set below the site line from Mumbles Road, thus minimizing the impact (see drawings above).

To ensure that users are not tempted to be dropped off outside the site, there will be a low hawthorn hedge running along the road side of the site preventing people accessing it directly from the road.

Part 2

Our vision

Who is it for?

The primary intended users are residents of the MCC area and ensuring it is accessible from across the area was one of the key requirements of the location. Many people, including children from three of the local primary schools, are within walking distance and its location on the prom will also allow for people to safely reach the site without having to walk or skate along busy roads. There are also bus stops on Mumbles Road and a zebra crossing allowing easy access by public transport from Newton and Mumbles. The Land Train also passes by during the summer months providing access from Mumbles and St Helens prom.

It should be noted that facilities such as these are not solely aimed at older children and young teenage boys. Wheeled sports arenas attract users from every demographic including wheelchair users who can interact on positive and equal terms with non-disabled users. Popularity of scooters remains high enables first time Skatepark users to explore the use before moving onto skateboards, BMX bikes or inline skates. There is also a vibrant scene within the UK for more mature users who have recently rediscovered “skating” with the advent of high quality wheeled sport arena provision and finding people in their forties and fifties riding the park is not unusual.

The accessible location will attract users from further afield and public transport from the City centre and parking at Blackpill will enable Swansea residents and tourists to reach it easily.

Social Impact and Safety

Well documented evidence from Police forces around the country attest to the fact that wheeled sports facilities such as the one under consideration are a contributing factor in reducing youth nuisance crime figures and developing social inclusion amongst a group of people who could be otherwise viewed as outsiders. Young people will see it as a meeting place where they can ride the park, chat and generally ‘hang out’ in a place they can call their own.

In addition, the provision of such facilities sits well with the Government’s strategy of getting young people out of an indoor and on-line gaming environment and into real outdoor physical activity. This supports the **Sport Wales Community Sport Strategy 2012 - 2020** as well as **Swansea Public Services Board, Local Well-being Plan** which has as one of its actions to engage the population in healthy outdoor recreational activities by providing them with modern and exciting facilities to use. Wheeled sports arenas are one of the few leisure activities which bring together all the socio-economic groups to play together.

A criticism often unfairly levelled at such facilities is that they will encourage youngsters to drink and take drugs. Anecdotal evidence suggests the opposite of this is true in a modern facility. The local police are fully on board and supportive of this project and will continue to monitor use of the site and deal with any anti-social behaviour should it arise.

The current Skate ramp has been in place for over 20 years and vandalism and anti-social behaviour has been minimal. The new Skatepark has safety built into the design in that there are no 'hidden' areas. It will sit between a busy road and the Promenade where walkers and cyclists pass by frequently. The mix of ages will also contribute towards the Skatepark being in the main self-policing and the newly formed Mumbles Skatepark group is primarily made up of older, experienced skaters who have taken it upon themselves to act as 'Guardians' of the Skatepark and will be regularly in attendance and plan to organise litter-picks, activities with the younger skaters and organise events and competitions.

Community and Economic Benefits

Feedback received by Maverick from other towns where Skateparks have been built indicates that the provision of a skatepark has a wider economic benefit to the area with all businesses near to a wheeled sport arena reporting increased sales. Past experience shows that a new park addition will attract "Skate Tourists" with a user demographic that will often mean they do not drive themselves. It is common for families to arrive in a town having dropped off the skater or skaters and spend the day shopping, sightseeing, eating or drinking and so on before collecting their child and going back home several hours later.

At a presentation of the Well-being Act held in Swansea in 2018, the presenter stated that skateparks were very good for the local economy. Where families have keen skaters they are more likely than not to choose a holiday in a location where there a decent skatepark for their children, while the rest of the family use other local facilities and attractions.

Insurance

Mumbles Community Council will ensure that all the necessary insurances are in place for the Skatepark and the land surrounding it.

Maintenance

Maverick will guarantee the Skatepark for 15 years and the design is covered by a £2m professional indemnity insurance. They will conduct an annual condition survey and will respond quickly to any reports of damage. During the first six months of use and on-going during the summer months, when visitor numbers will be at their highest, Mumbles Community Council will undertake weekly inspections which will be logged. During the winter months these will be monthly. There will also be £6,700 budgeted annually representing 2% of total replacement costs to ensure that any major landscaping and

repair/replacement work required can be fully funded. For full details see Annexe 7:
Maintenance and Guarantee Statement.

Costs

Minor repairs, graffiti removal and annual deep cleaning will be the responsibility of MCC. The only other on-going costs are grass-cutting, landscape maintenance and bin emptying. Swansea Council have quoted £3773 annually for this service, although this provided for 14 cuts per year. Due to the wildflower meadow grass to be used (Gower mix) it will only require cutting a maximum of 6 times per year.

SuDs maintenance is estimated at £1355.97 annually. Exact final annual costs should be available from Mr Dan McCauley, Swansea Council. Replacement costs are costed into the annual sinking fund which will cover all major repairs, replacement or refurbishment works during the lifetime of the project. The Sinking fund is 2% annually of total build costs.

We are confident that all the above costs will be covered by Fundraising (see below) but MCC has already committed to holding a contingency fund of £12,689 annually (adjusted in line with RPI) budgeted for within the annual budget of the council. Any shortfall in income generation to provide the gap funding of £1,256.67 (£7,256.67 in Yr1) will be met from the annual budget of the Community and Social Well-being Committee, or the Council contingency fund.

On-going costs per annum

Grass Cutting (Swansea Council contract 6 per year max)	£563.70
Bin emptying (156 occasions, Swansea Council contract)	£2,402
Annual hedge cutting and tree pruning (Swansea Council Contract)	£150.00
SuDs and Swales maintenance estimate (Swansea Council Contract)	£1,355.97
Annual Inspections (RoSPa Play Safety Ltd)	£400
Cleaning/painting/general maintenance	£2,000
Graffiti removal	£150
Sinking Fund representing 2.5% of build cost (expected minimum 25 year life)	£6,700
Insurance	£224
One off contingency fund to address potential parking issues (Yr 1 only)	£6000

Fundraising

MCC have already been approached by a local business interested in sponsoring the park. No figure has yet been agreed.

Local annual business sponsorship to include advertising opportunities on the park, social media and internet promotions linked to the park and naming rights for some events held at park.

All amounts are estimates at present but we feel confident these amounts can be achieved.

Main sponsor annually	12 x payments of £350
£4200	

Secondary sponsors (smaller one off support from local businesses)	
£1500	

Revenue from events (organised by the skaters)

Annual national event for x 100 participants at £5 each	£500
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3 smaller local events x50 participants at £3 each x 3 events	
£450	

Potential revenue total	MCC
£6,650	

See Annexe 8: **Mumbles Skatepark Association - Fund raising plans** for additional local fundraising initiatives

Our aim is wherever possible for the skatepark to be `self supporting` for costs and maintenance.

Revenue from Grants will also be explored.



MUMBLES

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Opening statement

This statement will explain in pictures and words how the development will fit in with the surrounding land and existing facilities, as well as the rationale behind the design process and the public engagement programme undertaken by Mumbles Community Council, along with the community and user group who will be helping to raise the money for the facility. Prior to arriving at this point in time.

It is part of a regeneration programme for the area as a whole, and specifically to improve the facilities for an age group who are under served currently in their field of interest. A by-product will be to widen the appeal of use to interested parties and to offer the community a modern and highly functional wheeled sport arena.

Its position sits on a grass strip that is between the promenade beside the sea and Mumbles Road. The site is already home to a Half Pipe and tarmacadam pad, which acts as Mumbles' only skate facility. Trees and bushes are scattered around but as we will later go on to elaborate, the plan is to incorporate many more to soften the visual impact of the new design.

In addition to the enhanced planting plan, a drinking fountain, seating, and footpaths with chicane entrances will be specified. In line with the guidance notes in the Pre-App return, the promenade will be widened where suggested to ensure compliance is guaranteed. Details of all of this will be provided within the relevant sections of this document.

The skatepark itself will be constructed from free form spray concrete with - in certain areas - narrow (25mm box section) galvanised steel edging to prevent chipping and it will be finished in a neutral and light grey colour but with flashes of colour for aesthetic purposes.

A spray concrete skatepark is significantly and demonstrably quieter in use than those built in other materials such as steel, wood or skatelite. Indeed, the existing Half Pipe falls into this latter category and will be decommissioned as part of the replacement programme. The new park itself will be sympathetically bundled allowing grass to grow up to the edge of the platforms and designed to take advantage of the natural topography which will further disguise its existence to other park users and members of the public not interested in skating



Design Process

The design process has been broken into the following headings for clarity

Assessment

Involvement

Evaluation

Design

This will enable planning officers to reach an informed decision on the public engagement process and the Mumbles Community Council's assessment criteria on the choosing of both location and the ultimate design.

Contextual Setting

The overall site covers an area of approximately **30,206 m²** which is predominantly given over to open grass land, trees and hedging, informal pathways, and an existing skate facility. The new wheeled sports park has a build area of only **748m²** (including the link paths)

Measure an Area



Area Output

30206.324 m²
0.030 km²
7.464 Acres
3.021 Hectares
325138.164 Feet²



Its relatively small footprint when taken as a percentage of the green space surrounding it, is viewed as being proportionate and in keeping by the council and will not impact adversely on anyone using the park land who has no interest in riding such a facility.

Far from it indeed, this proposal will improve the whole æsthetic approach of a skatepark within a green environment that shares a greater space with differing facilities.

Given there is a Half Pipe and tarmacadamed area already on the site, there is a group of youngsters who are already using the facility for what it is and are keen to see it updated.

It should also be noted that the relatively close proximity of the Blackpill Lido (opens in May and continues until September) and Swansea Bay Land Train (the latter runs from July through to September) fits perfectly with the demographic this wheeled sport facility is intended to cater for and likely to attract.

(Please refer to Appendices 1.0 – 1.7: the attached visuals for specific detailing on dimensions, footprints, access routes and heights etc of the elements that will be new)

Architectural Heritage

Llwynderw is a small area of green space which runs along what locals call “the prom” and is located between West cross and Mayals.

To the North of Llwynderw – approximately 1.5 miles - sits the Swansea University Campus and Singleton Hospital and sitting beside that, one will find housing that is typically, densely built, multi-coloured rendered two storey dwellings in a terraced format.

Around the centre of Mumbles houses tend to be a mixture of older, yet modernised and very much typical of seaside Welsh towns and villages, often rendered and painted in pastel shades with wood features. Some are even fully clad in wood and painted.

On Mumbles road itself in and around West Cross, the houses facing the seafront are sometimes quite substantial but often very well hidden with considerable amounts of planting which helps hide the houses when viewed from the road. These too are a wide range of styles and ages and the topography of this area of land has raised the majority of them onto a raised elevation in compare to Mumbles Road itself. Immediately opposite the proposed site, these houses are very much hidden by banked and planted terracing as can be seen by this picture.



Generally, there is a mixture of predominantly privately owned properties, as well as some social housing in West Cross offering a mixture of flats - including sheltered - bedsits and bungalows.

Blackpill Lido and Junction Café sit within walking distance of the proposed park on effectively the same stretch of land this replacement facility will be positioned, as does the Texaco Petrol Station and convenience store.

City and County of Swansea buildings are only 200m from the intended site.

With the area having so many architectural influences and the fact that we believe the proposed development will not be seen to be out of step or keeping with the area. We trust the planners will also view this submission in the same way.

Social Impact

Well documented evidence from Police forces around the country attest to the fact that wheeled sports facilities such as the one under consideration are a contributing factor in reducing youth nuisance crime figures and developing social inclusion amongst a group of people who could be otherwise viewed as outsiders. In addition, the provision of such facilities sits well with the Government's strategy of getting youngsters out of an indoor and on-line gaming environment and into real outdoor physical activity. This is set out in **Sport Wales Community Sport Strategy 2012 - 2020 as well as Swansea Public Services Board, Local Well-being Plan** It is this latter document that ties in the proposal at a county level, with the aspirations of engaging the population in healthy outdoor recreational activities by providing them with modern and exciting facilities to use.

However, it should be noted that facilities such as these are not solely aimed at 8 – 16 year-old boys. With the popularity of scooters remaining as high as ever, their use in such parks broadens the user base even further by enabling first time park users to explore the use before perhaps moving onto skateboards, BMX bikes or inline skates. Wheeled sports arenas attract users from every demographic; boys, girls, men and women, fully-abled users through to those less able, occasionally including wheelchair users who are often found enjoying the facilities and interacting in a positive manner. There is also a vibrant scene within the UK for more mature users who have recently rediscovered "skating" with the advent of high quality wheeled sport arena provision and finding someone in their forties or fifties riding the park will not be an unusual sight. This will also act as a draw to youngsters as a meeting place, to ride the park, to chat and generally 'hang out' in a place they can call their own.

A criticism often unfairly levelled at such facilities is that they will encourage youngsters to drink and take drugs. Anecdotal evidence in fact suggests the opposite of this is true in a modern facility. The local police are fully on board and supportive of this project and will continue to monitor use of the site and deal with any anti-social behaviour should it arise.

We have as just mentioned the direct support of local PCSOs who work in the area and we know that the Police Service throughout Dorset support the provision of such facilities and this is highlighted by the following appendices.

(Please refer to Appendix 2.0: Dorchester Police Press Release, Dorchester Skate Park and the subsequent drop in youth antisocial behaviour figures & 2.1 Letter from Inspector Fry)

Economic benefit to the Community

It is envisaged that with the provision of a skatepark it will prove to be beneficial to all the businesses that are near to the wheeled sport arena's location. We know from past experience that a new park addition will attract "Skate Tourists" and with a user demographic that will often mean they are unable to drive themselves. It is very usual practice for families to arrive in a town having dropped off the skater and spend the day shopping, sightseeing, eating or drinking and so on before collecting the boy or girl and going back home a number of hours later.

At a recent City of Swansea training course on Future Generations and Wellbeing, we were asked what we were doing to help our communities develop. Our MCC group answered that we are going to build a skatepark. The lecturer pointed out the usual more obvious points, yes it was good for the physical and mental health of those using it. But, more significantly, it was brilliant at bringing together **all** the socio-economic groups to play together. That last one according to our lecturer was a rarity.

What surprised us most was she went on to say that skateparks were very good for the local economy. In MCC research, Mums said that they always picked holidays with an eye to having a decent skatepark nearby for their children, no skatepark and the family choose somewhere else.

Even Cornwall and Devon know beautiful beaches and countryside is not enough to cut it, when you have a family holiday everyone expects a range of activities, and skateparks are on the top of their wish list. Cornwall and Devon have both invested heavily in skateparks because they know what it means for their tourism.

We know that a new skate facility increases visitor numbers by these skate tourists hugely and there will be a real and tangible financial benefit to the town once it goes ahead.

The rather unique spin off here is the fact that The Mumbles is already a destination for holiday makers, being the gateway to the Gower Peninsula's coastline and its fantastic beaches. The addition of a state of the art skate park can only increase the popularity of the area to potential visitors.

The provision of a new skate element will not increase the value of the land upon which it sits or is contained within per se, as it can only be used for leisure and recreation, however its provision will ensure the park is seen to remain alive and a vibrant part of the community.

In the Summer of last year, Maverick built and handed over a skatepark in Newquay. This was of a size that was intended to attract visitors and skate tourists from afar. We are led to believe from data acquired by the council that this facility is now the number one tourist attraction for the town.

While we cannot state this will be replicated in Llwynderw specifically – the respective sizes are quite different – we are confident in stating its provision will bring a tangible economic benefit to the town.

Of course we cannot discount the economic benefit that will be felt throughout the construction process, a significant amount of materials will need to be sourced locally and all the build crew members will stay as close to the site as possible bringing cash to the providers of accommodation and food and drink outlets.

There is also the potential for a youngster to be taken on locally to be given an apprenticeship experience with us. This is a scheme Maverick have run successfully elsewhere and will be explored directly with MCC.

Relevant Planning Policy Statements

Over the last two years Mumbles Community Council has undergone a number of major changes and improvements in the way we work. Every plan made is guided by Welsh Government's "Well-being of Future Generations Act." As is City of Swansea.

These are not just plans that sit on a shelf. MCC is pro-active in delivering the initiatives in our 5-10 year plan.

- For example:
We invested heavily to ensure the Santander bike scheme didn't fail.
- We're installing a covered bike hub to get families out of cars and cycling into Mumbles.
- We are providing a water filling station in Mumbles.
- We are working with local activists on pop-up-stalls to discourage the use of single use plastics.
- We help finance the regeneration of local parks.
- The skatepark is the biggest embodiment of all our principles in one project.

The following extracts from Planning Policy Wales (Edition 10, December 2018) (PPW) are of relevance:

General:

A Resilient Wales is supported by protecting existing communities and natural environments while promoting well connected infrastructure and facilities closer to where people live. Good housing, access to services, green spaces and community facilities help to create the right conditions for better health and well-being. A more Equal Wales can be achieved by recognising the strengths of existing communities and securing socially inclusive development, so they become desirable places in which to live and work for all members of society.

Coastal Areas:

6.5 The main planning principles for coastal places, which reflect the principles of Integrated Coastal Zone Management, are to support urban and rural development whilst at the same time being aware of, and appropriately responsive to, the challenges resulting from the dynamic interaction of natural and development pressures in coastal areas. Planning authorities should clearly establish what the coast means for them and develop, or apply, specific policies through their development plans which reflect the characteristics of their coastlines. For some authorities this may mean identifying areas likely to be suitable for development as well as those subject to significant constraints and considered to be unsuitable for development.

Supplementary Planning Guidance (SPG):

SPG relevant to the proposed development, which are available to download in full from the www.swansea.gov.uk website are as follows:

Swansea Bay Strategy (Adopted 2008) – places focus on a unified, high quality contemporary waterfront which extends from Mumbles in the west to Taw Riverside Corridor in the east. Policy BW1 focuses on creating a high quality, contemporary waterfront park between St Helen's and West Cross, which will become an important destination central to the bay.

The following guidance has also been prepared, but not yet adopted as SPG:

Swansea Seascape CBGSB Final report - provides guidance regarding visual seascape impact, setting out matters to be considered when appraising the potential effect of development in this regard.

The Development Plan for the City and County of Swansea (adopted February 2019) following policies within it have been interrogated and are mentioned as being integral to the importance of this proposal from a policy direction standpoint.

PS1 – Sustainable Place

PS2 – Placemaking and Place Management HC1 – Historic and Cultural Environment

SI1 – Health and Well-being SI2 – Providing and Safeguarding Community Facilities and Locally Important Uses

ER2 – Strategic Green Infrastructure Network ER8 – Habitats and Species

ER11 – Trees, Hedgerows and Development

TR1 – Tourism, Recreation and Leisure Development

TR2 – Developed Coast and Waterfront

T1 – Transport Measures and Infrastructure

T2 – Active Travel

T7 – Public Rights of Way and Recreational Routes

RP1 – Safeguarding Public Health and Natural Resources

RP 2 – Noise Pollution

RP3 – Air and Light Pollution

RP5 – Avoidance of Flood Risk

Policy TR 1 refers to 'leisure' based developments whilst TR 2 refers to the 'developed coast and waterfront' in respect of future development. TR 2 sets out a list of locations which includes 'Key destinations around Swansea Bay and states:

"Development of new visitor facilities and attractions, including proposals for sustainable recreation activities, will be permitted at the following coastal and waterfront locations, provided that they are of a scale and design that respects sensitive natural heritage, landscape, seascape and historic environment interests:"

'Key Destinations are referred to as being: Maritime Quarter/City Waterfront, St Helens, Sketty Lane, Blackpill Seafront/Lido and Mumbles Seafront, including Oystermouth, Mumbles Pier and Foreshore. The supporting text to the policy states:

"Between these areas the emphasis is on safeguarding and enhancing the environment of the Bay and other waterfront areas. Regard must be had to relevant adopted SPG documents relating to these destinations, including the Swansea Bay Strategy."

It can be noted that the Adopted SPG 'Swansea Bay Strategy (Adopted 2008), is now somewhat dated, however, developments for leisure/tourist related facilities envisaged in that SPG are primarily clustered together at Blackpill Lido and Mumbles.

In terms of other relevant policies Policy PS 2 (Placemaking and Place Management) and Policy SI 1 'Health and Well-Being' of the LDP are of relevance in relation to ensuring that developments enhance the quality of places and spaces.

Swansea Public Services Board – Local Wellbeing Plan: This builds on the Well-being of Future Generations (Wales) Act 2015 which has seven well-being goals, the third of which is termed "A Healthier Wales" – A society where peoples physical and mental well-being is the best it can be. A society where choices and behaviours that benefit future health are understood.

Three of the four key strategy areas in this policy are deemed to be particularly relevant:

Early Years

Live Well, Age Well

Working with Nature

Strong Communities

We find that this proposal sits neatly within these remits. The MCC Community & Social Wellbeing Committee (which is the steering group for this project) aims to provide opportunities for all members of the community to have access to and participate in activities that enhance their physical and mental-well-being. To ensure that all the work of the committee adheres to the sustainable development principles of the Welsh Government Wellbeing Act.

The key objectives identified are thus:

- a. To identify ways in which the Community Council can provide opportunities for all members of the community to participate in outdoor and indoor physical activities
- b. To work with local sports and social clubs to enable them to improve facilities and increase participation.
- c. To ensure that all sporting and leisure activities address the needs of all ages and abilities, particularly older people and those with disabilities

- d. To develop new ways of reducing loneliness and isolation within the community
- e. To work towards an integrated transport strategy that encourages more active travel, e.g. safe walking routes, bike hubs and cycle paths
- f. To explore the potential for community asset transfers of recreational and leisure facilities.
- g. To encourage positive engagement with young people and youth groups.

**Implementation of the Health and Wellbeing Strategy through skatepark provision:
A strong in mind and body future generation.**

Mental health and physical wellbeing in children and young people is increasingly being flagged up as a serious issue. This has been identified by CoS as one of their main concerns. Even with current CoS resources, plus the natural resources of the sea and Gower on the community's doorstep, disaffected youngsters is still an issue.

Research into the amount of exercise that youngsters should have has shown that we are facing an obesity crisis and will soon see the longer-term secondary impact on service provision, which will be beyond the management of the NHS.

A recent BBC article emphasised that more children are chronically obese by the time they reach secondary school than ever before. This was unheard of even a generation ago. Our children are not in a good place physically and this in turn can impact their mental health and wellbeing.

The modern world does not lend itself to the kind of nurturing that young people need, they are put under so much of the wrong type of stress, which fuels anxiety and worry which can have that distinct effect on mental health, along with their physical health.

We know that young adolescents need to get plenty of exercise, however, when they get to be twelve plus (secondary school age) sport becomes more competitive and schools, clubs and parents can sometimes replace the love and need for exercise with striving for excellence, or being part of a successful team rather than on the fun and enjoyment of just doing it.

Traditionally, organised team sports like football, rugby and netball start to be about how well you play, rather than having a kick about and a game with friends. Teachers can sometimes make matters worse by always needing to pick the most talented children for competition, leaving a large proportion of this age group feeling that sport is not for them. Skateboarding is an absolutely perfect replacement for such activities.

Our youth have also become more isolated than the last generation, in part due to the rise in social media and are far less likely to develop meaningful friendships and positive peer groups.

An antidote or inoculation against this could come from a skatepark- a shared outdoor space where they can feel welcome and safe and are allowed to be themselves whilst still challenging themselves. Engaging in physical activity, which is both challenging and social,

gets them outdoors, in the fresh air and away from computers, social media the confines of home is a positive. We found during our extensive research that skaters of different ages tell many stories, give many personal accounts, of youngsters who found a home, a safe environment to grow up, within the skating community, which they hadn't found either at school or, unfortunately, at home. We were unashamedly told that this peer group and the advice and company of older respected skaters had, quite simply, saved them. Respect and trust were words used again and again.

Our PCSOs gave MCC perhaps the most telling piece of information. The youngsters they speak to have revealed that what they mainly want is a safe place to meet. A place to feel safe.

The NPPF interpretation

The National Planning Policy framework maintains the 'town centres' first approach, which means that new shops and leisure developments should look for sites in town centres first which this proposal clearly fulfils as ensuring the vitality of town centres and promoting healthy communities form key indicators to achieving sustainable development.

We hope to prove throughout the rest of this document, care has been taken to ensure the overall design fits in with future and existing planning objectives. An aesthetically pleasing addition to the park that enhances existing leisure facilities can only be viewed as being beneficial for Mumbles and we trust you, from a planner's perspective, will see it in the same way.

Planning permissions in place around the site:

We have carried out a search on the Swansea Planning Portal and the latest significantly sized housing planning application we were able to find dates from March 2015, (2015/0600) which was Pre-application for the construction of 10 dwellings at 310 Mumbles Road, which is 1km to the South of the site. With no further applications from the applicant apparent and the notes from Highways indicating considerable changes would be needed to support this proposal, it is therefore deemed to have no impact on our proposal since it appears to have been abandoned.

There are additional applications for extensions and smaller works in West Cross, but once more we cannot see any of these being detrimentally affected by this proposal

We are also aware that a new carpark on Mill Lane – just up from Clyne Gardens – has just received planning permission and this is easily walkable to the site.

Involvement

Consultation Process

The desire from local riders for a replacement facility for the existing mini ramp / half pipe has been known by MCC for many years but the consultation process has really only in the last two years really come of age.

This coincided with the grateful involvement of a group of more mature and very motivated riders who really wanted to make this project happen. It was the engagement process that was run by them in conjunction with MCC that has led to the design presented in this application.

It started with a PACT (Police And Community Trust) meeting, where there were complaints about some teenagers causing trouble, hanging around and damaging property in the Mumbles area. The question arose - what can we do, as a community council, to appease the adults, distract and engage the youngsters and turn a negative situation into a positive one?

We started to research and found out that the Police see skateparks as a force for good and one they can easily engage with. They can pop in during regular patrols, talk to the young people there, watch the skateboarders perform and get to know young people in a way that is not confrontational. They can offer advice and get on first name terms, which is a positive for both sides. Not surprisingly, there is documented proof, that a well placed, well designed, visible skatepark lowers the youth crime rate, along with a decrease in anti-social behaviour. So from the start, for these reasons, the need for a skatepark seemed an avenue for us to explore.

MCC immediately sought the advice of the top skatepark builder in the country. They did a site visit at Llwynderw and advised that the half-pipe on the site is past its expected lifespan and needed to be replaced as a safety issue. They had no financial incentive for this appraisal. It is their professional, expert opinion.

The meeting in the Ostreme centre

MCC had thought that it might be wise to ask a few adult skateboarders to our Community and Social-wellbeing Committee Meeting, so it sent out invites to some of the local skateboard and surfing community. We were amazed at how many people responded, saying they would most certainly turn up to have their say. We quickly realised that we would need a bigger venue than our little Community Council Office, so we booked the Ostreme centre. The room was packed out. The meeting was an eye opener, here were so many adults, saying that they travelled great distances from Swansea and Mumbles to Skateboarding parks across the country. They have children who also skateboard. They travel across the country, choosing to spend money elsewhere doing this because there is nothing here for them. This is such a missed opportunity for the community and commerce.

We talked to many groups of older citizens who use our community centres. They were instantly supportive. "Give them somewhere to go and something to do."

The consultation process covered every known aspect of skate park provision; location, style, heights and make up of park features as well as including items that were seen to be desirous by the council. The latest of which was the water fountain.

Why Llwynderw?

Siting the Skatepark at Llwynderw is not an arbitrary choice, it is a well researched decision. When deciding the most ideal site for a new skatepark MCC identified and researched all possible sites within our boundary. We and the users wanted a site which is accessible on foot, by cycle or skating. We acknowledged that parking needed to be addressed.

Present Skatepark site at Llwynderw

This is a tried and tested site. There is no history of vandalism or poor behaviour.

Our tender document stated:

After extensive public consultation, MCC has identified that it is looking for an exciting and dynamic Skatepark, suitable for skateboards, skates, scooters and BMX use, in the area of land immediately surrounding the existing half pipe and asphalt 'basketball' area, between the main road and the promenade path.

The Skatepark should have a design that is workable and yet distinct and represents Mumbles and the history of wheeled sport in our area. The design should also be sympathetic to the beauty of the area and the views, enhancing and adding character to this area of the promenade.

We made it clear that the design should be zoned for the use of experienced users, learners and young children.

Alternative site considerations:

Langland tennis courts

A motion was passed by MCC that they would only fund the refurbishment of the tennis courts for tennis. No other sport or activity. So that was out.

Underhill Park.

The planned development of Underhill was already about 4 years in discussion with no sign of a plan or lease. We decided that we couldn't wait another 2 years for them to get organised. During this time with joint meetings of MCC there was never any suggestion from the body organising Underhill that they wanted the skatepark to be considered for their site.

Limeslade carpark

The car park is not owned by one single body. Negotiations would have been difficult plus the fact that it could possibly mean increased traffic through Mumbles, which is what MCC is striving to address and avoid.

Mumbles Tennis Courts

These had to be ruled out because of the uncertainty of future uses.

Village Green at West Cross

Local councillors are already in talks with the local business community about building a park in front of the shops. These are the people traders want to attract. Residents above the shops and further along the road do not want a skatepark on the green. There is also a large gas pipe-line which runs across the site. It is therefore not possible to sink a bowl into the green.

Blackpill

The sea-ward piece of land, opposite Blackpill Garage was discussed. At first glance it seemed a good location. Away from the road and good sight lines. However, this is part of the Blackpill nature reserve. It is a safe place for Ringed Plovers and Oystercatchers. It is partially a triple SSSI site. Local naturalists made it clear that they would mount a lengthy campaign against building on this site. We canvassed widely with the families who use the Lido. We repeatedly heard, "It's an accident waiting to happen." That area is designed for tiny tots and buggies, prams and tricycles. Families were very pleased that a skatepark was going to be built and very happy that it would be sited away from the toddlers, as they both need their own space.

MCC sees no point in asking the public for practical advice and then blatantly ignoring it.

The tender and wider engagement process

After putting out the tender and reading the subsequent plans and proposals, we were assured that all engineering difficulties could be managed. Now was the time for finding out what the rate payers of MCC had to say.

MCC realised that it had to set in place serious research vehicles to support the serious expenditure of public money. And test the public response to the Llwynderw site.

We used Survey Monkey for an initial canvass. This allowed us to ask people in our area what they thought of this particular idea and site and to find out how popular they were, or not. Respondents had to put in their postcode so we knew they lived in the area.

The result was astounding, a high percentage were in favour of a skatepark, they were also asked the questions, as to whether their friends and family would use it and also, did they think that the area on the seafront by the current skate ramp/ half pipe, would be a good site for a renewed skatepark. The answers to all three questions were a resounding, "Yes!"

In a one-week period the survey gave us:

48,224 Total Reach

1,849 Reactions Comments & Shares

15,918 Post Clicks

4,738 Photo Views

11,180 Other Clicks

Continuing our consultations, we went to speak with the young people in our local Junior and Secondary schools to find out what they thought.

For example, Mayals school was asked if they could ask their children's council the same questions and give us the results.

The Headmaster responded. He explained that this was not good enough for the children as the children wanted to tell us themselves what they wanted from the skatepark and made it clear that they wanted to be involved in the project right from the start. They were asking for ownership of and partnership in the project. This was echoed in school after school.

And so it began. St David's school had the same attitude, yes, they would use the park and they had suggestion and designs for us. On visits to Llwynderw and Whitestone, they were passionate about a skatepark, they had specific designs they wanted and they gave us so many drawings! We promised to pass them on to whoever designed the skatepark. During the visits the teachers were so positive about a skatepark as a marvellous way to get children to do a physical activity they enjoyed that could become a lifelong activity. Grange School took over a complete Assembly time. Teachers remarked that children who habitually make no contribution to a discussion became involved. Parents who indirectly found out about the research began contacting councillors to make sure their voices were heard. Teachers and parents wanted a skatepark, they see it as a great addition to our local area.

All of the Secondary schools were contacted via Governors and teachers and all returned completely positive responses.

Going back to the parents and teachers who spoke to us, they said their children would gladly skateboard, skate, BMX, or scooter for hours, if they just had the space to do that. Mayals school were very excited about the prospect of taking their children down to their skatepark every Friday afternoon as part of their activities agenda. Suddenly, the amount of physical activity a child needs to achieve each day to be considered fit, could be within easy reach. This is one of the school's guiding principles, echoed in all of the schools, of helping to develop strong, healthy confident children and young people.

Detailed design meetings

The user group were also led towards a design that would be seen to be aesthetically pleasing to those in the vicinity who would have no interest in riding it themselves. At all stages of the design process this core group were involved in key decisions and kept informed with updated information that would influence the design.

As the design developed over the years, elements were incorporated or enhanced to ensure the park was going to be as widely accessible as possible to as many people as possible. This naturally is including those who are less able and would want to use the facility in their wheel chairs. It is this latter design requirement that led to the final design amendment at the suggestion of a young Welsh girl who is the current world "Wheelchair motocross champion" which would enable wheelchair motocross (WCMX) riders to be able to use the skatepark at Llwynderw and that was the widening of a number of platforms that would make using the park from a wheelchair user's perspective, easier.

A further example of this would be the decision to alter both paths from the park leading to the promenade and including chicane entrances. The group embraced the changes, immediately understanding the rationale behind their inclusion.

The engagement team has performed in an exemplary manner, the users genuinely feel like their views have been listened to and delivered upon and this is always key to having the main players in the design group emotionally engaged with the project. This is sometimes referred to as having "perceived ownership" of a scheme, and in this instance, it is exactly this that has happened.

Evaluation

It is clear from the consultation and engagement process that a mandate for the facility was set. Taking stock of the original idea, there are major positives for the welfare and well-being of the community, in that:

Police, private individuals and parents want an open access resource that will appeal to a broad section of the community and include young people who might drift into antisocial behaviour due to boredom and inactivity and the lack of a positive peer group and older experienced role models.

- Parents, teachers and children identify the need for access to a free, open air sports facility that can be used socially by individuals, families and groups.
- Skateboarding and wheeled sports (BMX, skating, and scootering) can provide individuals with self-improvement goals in both competitive and non-competitive arenas.
- A local skateboarding charity has offered to work with youngsters who would not normally have access to the sport or the equipment.

The additional benefit of a skate park:

It's an excellent place for young people to meet each other. It is now well documented that Social media is not the place to make safe, strong friendships.

Teens are shy, they can find it difficult to string a sentence together when strangers are involved. This means making new friends can be extremely difficult. Friends made on social media are not in any way as useful as physical tangible friends you can meet enjoying a shared activity. Boys and girls can find the whole interaction thing excruciatingly embarrassing, that's where a space like a skatepark comes into its own.

They can stand on the side-lines, in fact they have to wait their turn, they can talk about the person skating they can talk about their rig, they don't have to talk at all they can just be. This is an incredibly important resource, there are not many such spaces that offer such an easy opportunity for friendly social interaction. Yes, you may get camaraderie from other sports, but there's always the shadow of "them and us". The competition being the other team. The competition with skateboarding is more aimed at yourself.

It is supportive and empathic. One boy said during the engagement process, "I don't laugh when someone falls, 'cos I have fallen in the same way, we have the same bruises". This defines empathy and resilience; and explained in one sentence.

Location and construction

To locate the facility in a position and place that was acceptable to all – this includes residents who may not use that facility, but those who could potentially be affected by its provision. To make it multi-functional to all wheeled sport disciplines, for it to be made of free form concrete to facilitate the smoothest of rides and for it to be the quietest of mediums to be built from.

The design had to be aesthetically pleasing as well as reflecting the wishes of the user group and the execution of the build was to be done by the industry leader.

The council wished to make the design unique and aesthetically pleasing to both the skate community and any other park user. Fortunately, with the budget and space available there was little in the way of compromise that had to be resolved amongst the stakeholder group and the end design reflects the fulfilled wishes of them all.

Mumbles is strongly defined by our surfing, and skateboarding culture Beach Life!

Growing up 20 years ago, beach life culture influenced all of us in how we dressed, where we bought our clothes (Dave Frier)! It gave Mumbles its identity. It lent Mumbles a much needed glamour that we seem to have lost. This culture is still with us, it's just been pushed underground. We only need a really good skatepark to showcase it again. You only need to look at Cornwall and Devon to see how they embrace the skateboarding/surf boarding culture and how it pays them to do so.

Skateboarding will be an Olympic Sport in 2020

Swansea really could get some homegrown talent simply by giving the skateboarding, BMX community much better facilities. James Jones is a young man from Swansea. He is in the British Olympic team for Tokyo 2020. He had to leave Swansea to go where there were better facilities. It's sad he couldn't have stayed closer to home, and inspired others. He drove a 5 hour round trip to be at the Ostreme meeting and show his support for this project.

Self-policing spaces

MCC has been asked how it foresees the management of the site. There is not a single solution. Experienced skateboarders co-opted onto the MCC Community committee have advised that:

Experience has shown them that there will be self-management of the site. It is intrinsic to the design that vandalism of a concrete skatepark is not usual but can be managed through setting acceptable codes of behaviour and implementing them through self-appointed park "rangers" drawn from the user group themselves who are known and respected members of the riding community.

Skateboarders themselves will ensure that the site is respected. Any inappropriate behaviour is, apparently, quickly discussed on social media, so positive peer pressure will keep the site safe.

In Haverford West there is a "Gentleman's Club" of senior skaters who oversee or manage the site. We have been assured that a similar arrangement will operate at Llwynderw. We have seen in many meetings that mature skaters have total respect.

Local Junior school pupils have asked that they design signage which will give, "Do's and Don'ts". They are already feeling ownership of the site.

Parental involvement. Mums have offered to run a rota at weekends. There will different times dedicated to different ability groups.

PCSO's have agreed to and will help patrol the site.

MCC have involved every Junior and Senior school in the initial survey. Every response has been positive. Youngsters are already taking ownership of the project.

The present Llwynderw facility is not vandalised. There is no history of misbehaviour or misuse.

MCC will inspect the site weekly. A start up patrol could be funded for the first 6 months when usage will be at its highest.

Landscaping will provide lines of sight from every angle.

Regular maintenance of the landscaping and the regular emptying of waste bins will set the standard for the expectation that the site is to be respected.

When many different skaters were asked for advice on how best to "police" this space, they all answered in much the same way. We, the skateboarding community police it. All they ask is to put the new park where the skateboard ramp already is. They point out that in all the years that the ramp has been there, there have not been any problems.

It is between a busy road on one side, and the Seafront path the other. It makes the young people feel safer. Yes, it's enlightening to learn that teens fear crime, and being victims of crime. When there was a large scale questionnaire put out to teens and young adults about the sort of spaces they wanted to interact in, the most important point that kept coming up was safety and wanting to be in plain view of others. They don't want to be hidden away; they feel safer in plain sight. We can do that for our children we can keep them under our respective noses where they want to be.

Whole family activities is MCC's plan. Decent seating and a well planned space makes for a happy destination. A community space to bring the community together.

MCC is already pursuing an agenda of Wellbeing by funding bicycle racks and water filling stations in Mumbles. MCC is against single use of plastic bottles and encourages and supports sustainability. Therefore, there will be a water filling station on the site. There will be cycle racks to facilitate skaters and families who want to stay off the roads. There is a succession of outdoor gym stations along the prom from Ripples Green which makes an exercise highway up to Blackpill Lido. The skatepark is within walking distance for local schools.

A CoS officer made the point after the pre-planning application that he was worried about skaters doing tricks "5 foot in the air" and causing traffic holdups and accidents. As the plans show, the bowl is placed down the slope, away from the road and is sunk into the ground. We have been advised by other relatively new skateparks such as Porthcawl, that there is increased interest for the first couple of weeks but that soon goes back to normal and further questioning of the proposed supplier leads us to believe that they cannot find evidence of a skater performing a trick in a skate park that has led to an accident on the road near to it due to the driver being distracted.

There have been concerns raised by some residents that there will be increased noise. Tests have shown that the new skatepark will be less noisy than the old half-pipe and that the level will be lower than that of the ambient noise of the traffic on the road.

Design

Interpretation of the design brief

To fulfil the design brief there were definite parameters that had to be met. The scheme had to be functional above all else, it has to work as a skatepark, or it will become a white elephant. Secondary to this is the overall look. To reduce the impact of the main body of concrete within a green park environment it was designed so the upright units facing the houses on Mumbles Road are bunded and will be seeded. Naturally this effectively hides the unit when viewed from a distance and all that will be visible will be low grassy mounds interspersed with patches of concrete.

To illustrate, here is a picture of a similar Skatepark in Henley-on-Thames.



The concrete is uncoloured and once cured presents a light grey appearance. There are elements of bright steel work within the park which are there to protect edging and avoid chipping when the park is in use. You can see where by inspecting the attached visuals.

Implementation of design - Risky behaviour and modern Gladiators

There has been research done in the field of Play Development, that shows that children need an element of risk in their play for their young brains to develop properly. Play that involves balance is particularly useful in aiding their brain's growth.

When children engage in any skateboarding, scootering, skating activity, their ability to concentrate for longer periods of time has been recorded. This has been seen with children with learning difficulties, and emotional developmental problems. Skateboarding seems to encourage a type of active mindfulness, not unlike that caused by Yoga. Unlike Yoga, the element of risk is an important part of this process. The adolescent brain needs that "hit" of fear, and confrontation they get from doing their skateboarding tricks, which leaves them calm, and clear headed, the full concentration when they need to fulfil the trick, completely clearing the mind of anything else. Active meditation no less!

The challenge they are up against is simply themselves, but they are also performing for others and they want to be seen, this is incredibly important for young people, this chance to shine in something.

One young man who contributed to MCC's research said that skateboarders only take on air, they want a bowl that will be exhilarating. When asked if it was his own Coliseum he laughed and said, "We are all Gladiators!"

Ensuring Site Cleanliness

MCC have already taken on a number of cleanliness and hygiene contracts which would otherwise mean local facilities would have had to close.

MCC will provide funding for suitable bins and would work with CoS to come to a suitable costing for any additional need.

Whilst MCC recognises that CoS is under tight budgetary constraints at present, it needs to be noted that the facility is 'added value' for the Local Authority and as such it respectfully reminds CoS of the shared responsibility to residents who pay their Community Charges.

(See Appendix 1.0 – 1.7: Park Visuals)

Engineering Detail Statement

Additionally, you can see below an example of a typical concrete element with the protective steel edges shown. There will be similar construction methods within this design.

STEEL STRIP EDGING DETAIL



We have addressed drainage by utilising positive drainage straight into the surrounding edges of the street section of the skate park and a suitable SUDS compatible drainage system will be provided for the skate bowl which will be illustrated in the following and additional SAB application.

We have however, had the site surveyed to allow us to understand the topographical layout which has shaped our initial drainage solution when taken in conjunction with the Geotechnical Survey Report and the FRA which have both been supplied.

The suite of construction engineering drawings will be commissioned once planning permission is secured and our engineer's expertise on all matters pertaining to successful drainage design will be employed to the full in the creation of said drawings.

The Design Detailing Explained

The design features all the elements that the user group wanted to see in a skate park. The idea behind the facility is that the routes around the park link up so they can work as one long flowing run. It will be fast and primarily a huge amount of fun to ride. Its design compliments the existing skate park and offers a completely different style of ride to anyone using it. The facility has been designed to fit with the existing landscape and fit in with the overall aesthetic. The benches and bins have been included as part of the design and are positioned off the main runs yet remain sited to be RoSPA compliant as well as being positioned to be in prime viewing positions.

It is important to bear in mind the facility has been designed to cater for ALL skate disciplines and skill levels. It will also encourage use by those of a less able disposition. It is an all-inclusive design in every sense of the word. The park has been designed to be built up and to fit within the existing site. The surface of the skate park will be finished so that it is quiet, smooth and fast, yet retains adequate grip for skate and BMX.

Safeguarding and Welfare considerations

MCC is very aware of the need to safeguard more vulnerable children and adults alike. Everyone will be in plain sight of joggers, cyclists and walkers with an obvious demarcation of the skating area. Seating and a picnic table for family members near the children's zone will also be provided

Safety of participants and users of the Prom

Local schoolchildren and the skating community have already asked if they can design boards with Do's and Don'ts.

There will be a fence with a Hawthorne hedge which will be a border next to the road for the length of the Skatepark to prevent access from the road, so there will be no dropping off from cars on the road. The abundant shrub boundary of the adjoining property making the natural boundary on the Mumbles side. The path from the children's sector will only give access to the prom via a hooped gate. Skaters will have to stop to be able to access the prom. The path on the other side will be punctuated by a disabled hooped access gate. Again, users cannot skate straight onto the prom. This will ensure a slow exit and remind users of the change of pace of prom users.

A physical landscaped and planted boundary between the paths will also discourage access directly from the prom between these two entrances

The well thought out design is there to act as a marker to proclaim the facility as one the town is proud to own, is unique to the area and one the town would want to promote.

This facility should be seen as a public space available to all and a great addition to the community.

MUMBLES SKATEPARK DETAILED DESIGN STATEMENT

The design for the Skatepark has been drawn up in line with local user group requests and represents their ideas in tandem with fiscal and spatial constraints set by the council.

For a detailed statement on the thinking behind the physical design and functionality of the park please refer to: **Appendix 3 Skatepark Design Statement**

Access

Access by public transport

National Rail Network Map

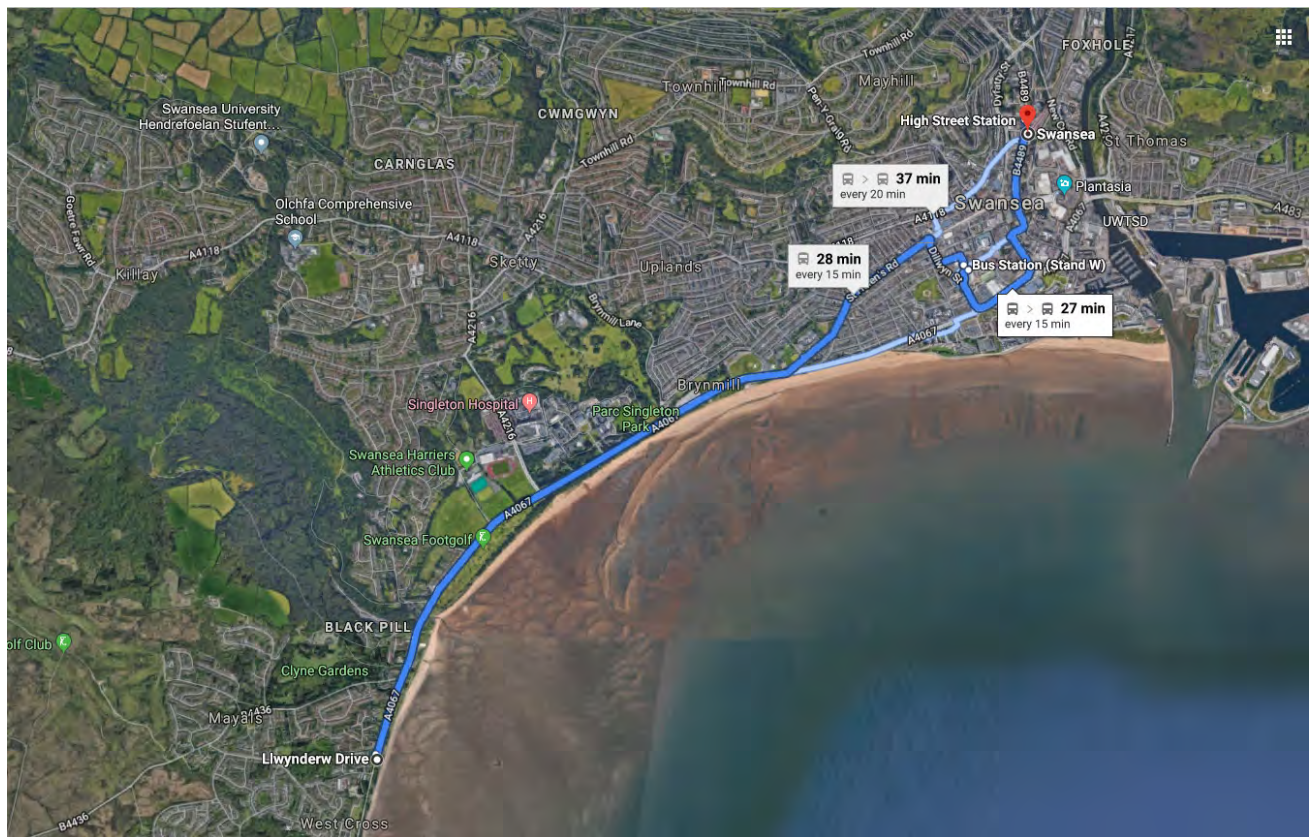
As can be seen access to can be gained by the national Rail Network. Cross Country and Great Western Railway hold franchises that will provide the service for visitors to **SWANSEA** (the closest train station). The simplified map below shows how users would be able to visit the area from any part of the whole country if rail is their preferred method of transport.



The train station nearest the site is **SWANSEA**, buses go to and directly from the skate park. The stop **Llwynderw Drive - stop id swapjwa** is approximately 150 m from site. The journey time is approximately 27minutes being roughly 3.6miles between the two destinations.

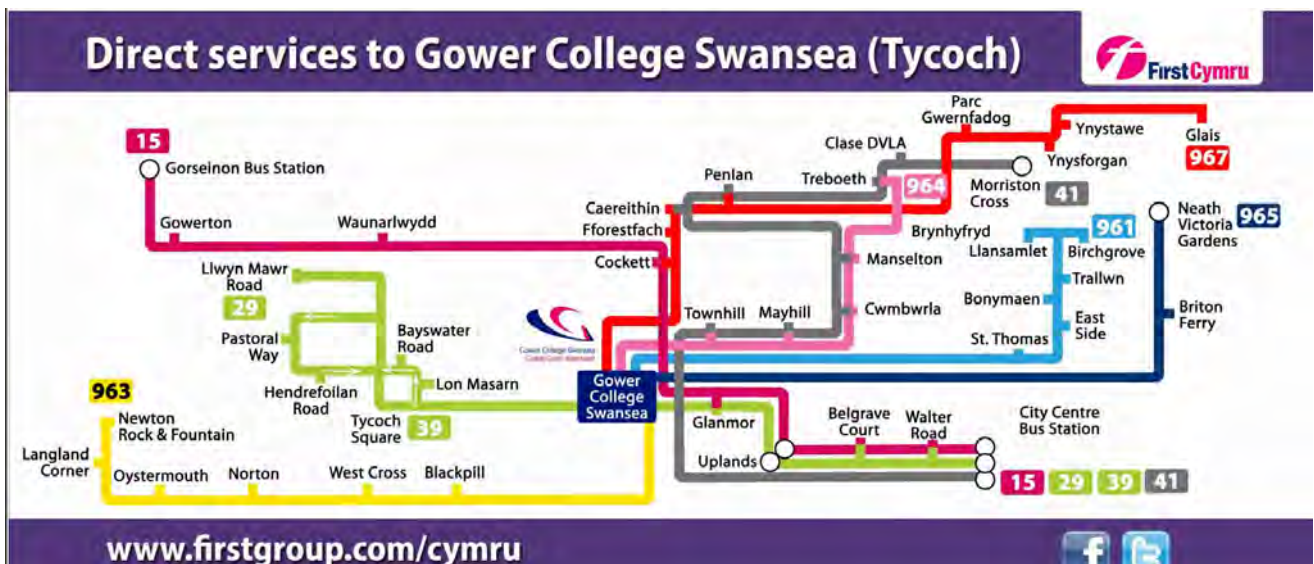
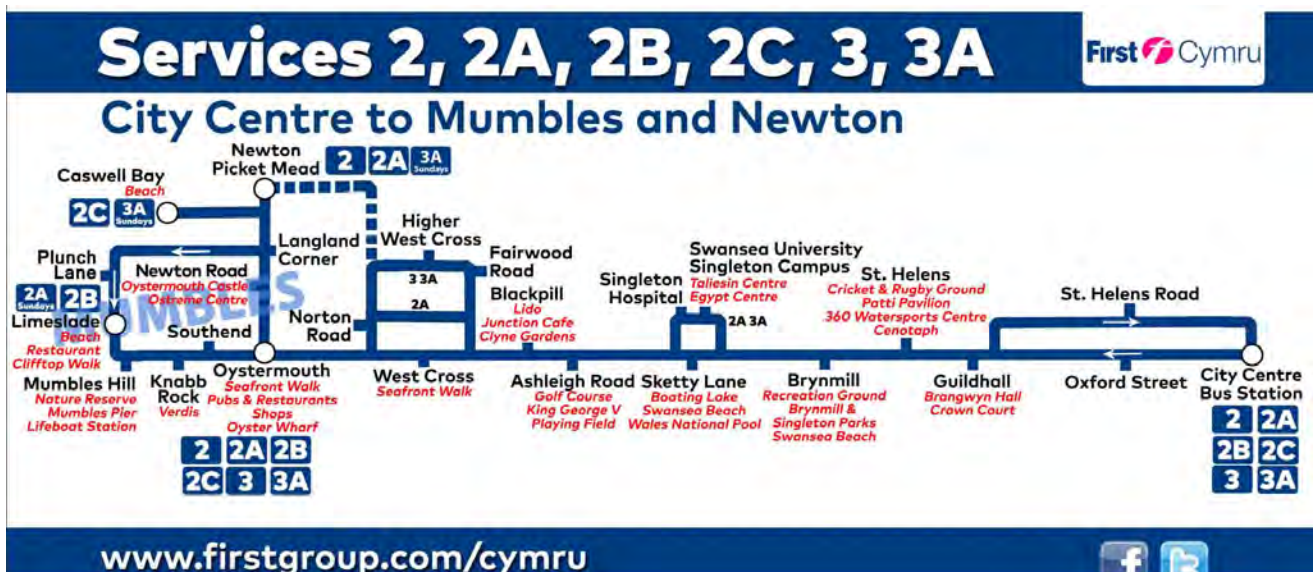
Buses leave on average every 15 minutes.

The services 2, 2A, 2B, 2C 3A, 14A and 963 all travel to this stop.



BUS ROUTES TO WEST CROSS, SWANSEA

First Group hold the franchise for bus routes in and around West Cross. While it is possible to access the intended site from pretty much any point in the UK via bus the reality is, we do not expect visitors to use this method from any further afield than say Llansamlet, Fforest-fach or perhaps Llanelli. Therefore, the schematics below will reflect this.



Access by car and car parking

There are two obvious car parks for those wishing to arrive by car and it they are indicated below:

Blackpill / Clyne Gardens Car Park

description

Mumbles Road, near SA3 5AS

- 138 spaces
- 9.00am - 7.00pm
- Barrier: No

Park here for Clyne Valley Country Park, Clyne Gardens, Blackpill Lido, Promenade and Swansea Bay, Places to eat

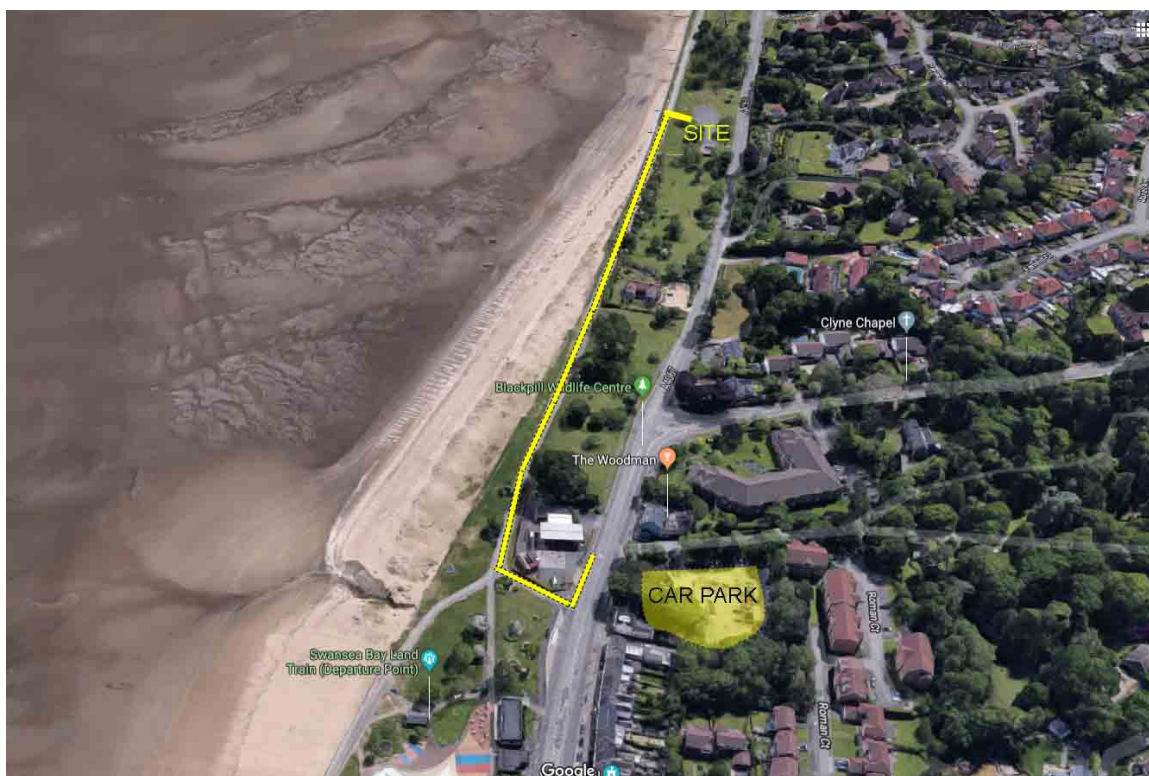
All day transferable ticket £5

Charges - £1.50 1 hour, £3.00 2 hours, £5.00 all day, Blue Badge concessions available

This car park is a distance of approximately 730m from site which could easily be walked or a combination of a very short walk to the post office bus stop on Mumbles Road and then catch a bus for one stop to Llwynderw Drive bus stop. The reality however is, we do not believe the bus option will be taken given the distance is so short.



There is a smaller car park nearer to site, but we are unable to find a note of the car parking charges on line. It may be referred to as the Woodman Car park whose entrance is directly opposite the Petrol Station and The Woodman PUBLIC HOUSE. The distance from site is approximately 440m



Additionally we can see there are opportunities to park on the street on roads that sit off Mumbles Road. We can see no obvious restrictions on parking here but clearly any spaces would primarily be aimed at the residents who may live opposite.

Additionally, as mentioned before, there is planning in place for a new carpark on Mill Lane which is well within walking distance and on the City of Swansea's website giving information on the existing half pipe, they suggest parking at Singleton. We however believe this is unlikely to be used given the closer options users can choose from.

Parking is not an option on Mumbles Road itself, we are also aware the TRO's are expected to be placed along Mumbles Road close to the proposed facility forbidding either stopping or parking.

Public information on parking

A site map will be widely distributed and publicised. Visitors will be directed to access the Skatepark on foot, bike, scooter, skateboard.... with clearly delineated bus stops and paths down onto the prom.

The map will also highlight current parking areas and discourage parking in residential zones by informing visitors that MCC can fund a regular traffic warden (although this has not yet been delivered by CoS)

There will be a significant use of signage to direct users to local parking and inform on the consequences of prohibited parking.

The 2 nearby bus-stops have tarmac paths with direct routes to the prom. There are crossing points at West Cross, Llwynderw and Blackpill near these bus stops.

The natural dropping off points will become Blackpill and West Cross. There are already designated crossing places that can be accessed by pedestrians wishing to cross the busy road. The bus stops nearby have pathways to access the promenade. There are crossing points at West Cross, Llwynderw and Blackpill near these bus stops

Our co-opted skateboard users on MCC's Community Committee have informed us that they are accustomed to having to "park and walk" to popular skatepark sites. Cardiff has a 15 minute walk from the carpark, Haverford West has a 10 minute walk. Signage will direct skaters to carparks at Singleton, the new carpark at Mill Lane or the Park and Ride at Swansea Rec. and so on. City of Swansea's own website already directs skaters using the Llwynderw half-pipe site to Singleton carpark.

Mumbles is already a popular destination; however, although an initial concern about parking is likely, educating the Skate community and the local community will be key in avoiding initial opposition.

We recognise that the park could become a draw for competitive and Skate Jam type events, if the facilities are popular and meet the standard we plan to attain.

Therefore, MCC will seek to work with CofS to facilitate the use of park and ride facilities. MCC will be guided on how such events are costed and organised, for example, on the Big Weekend event – particularly the use of the University sites at Ashleigh Road and the campus.

In addition to significant publication online and the likely use of social networks to advertise the park, MCC would also provide a press release and work with schools and local clubs to educate and assert the need for planned and considerate use of the facility.

There are already designated crossing places that can be accessed by pedestrians wishing to cross the busy road to gain access to the park once they have parked their car and walked to site.

Given the choices available to users who intend to arrive by car, we would suggest there will be ample spaces for those who do, especially given that the typical demographic of the facility user would be that of someone who does not yet drive.

Access by foot

The new facility can be reached easily by foot from many directions and access points along the way, but we would expect nearly everyone who chooses to walk to take advantage of the Promenade path beside the sea wall which stretches from Mumbles Pier all the way to Blackpill Car Park, a length of nearly 4kms.

Mumbles Road, which broadly matches the route of this path only has a pavement on one side and that side is opposite to the side upon which the land the skatepark will sit.

There are formal pedestrian crossings in place that would enable users to cross the road in total safety, the nearest of which is only 150m to the south of the site. They would only need to cross the open grass land – a distance of about 30m - after this to arrive at the Promenade path.

The existing link path from the Promenade to the skatepark will be enhanced and a second link path will be created, both of which will have chicane entrances, and these paths will encourage visitors to arrive from this direction.

We are certain that the Promenade Path is already accessed at multiple points in this manner from Mumbles Road by pedestrians, so we can see no issue with this method of access.

Additionally, part of the design process has placed both an enhanced hedge line and low fence – on the skate side of the hedges – along the green between the skate park and Mumbles Road. The rationale behind this is to discourage either pedestrians from accessing the park directly from Mumbles Road at that point, and to also deter visitors being dropped off by car and stopping opposite the site.

We understand TRO's will be put in place along this stretch of road to formally stop this from happening.

Details of the pedestrian access routes, new and existing footpaths, hedges and fence lines are shown in the attached visuals.

Access for construction traffic

There is only one viable route into the skatepark, we will need to access via Mumbles Road since utilising the Promenade has been ruled out on grounds of both safety to the users of it and potential damage to the surface of the Promenade itself due to the weight of the vehicles we will need to use to deliver the project.

A temporary trackway system (as indicated below) to the site to ensure a problem free delivery will form part of our TMP.

Below is the anticipated entrance to the skatepark site



Schematic showing point of Construction Traffic entry and routes to site areas

As can be seen by the visual below we intend to access the site from an angle. The reason behind this is to ensure we will not block the road even temporarily by forcing delivery vehicles to approach the site from a right angle. Access and Egress will naturally be controlled by trained banks men. No new permanent paths or roads will need to be created to effect delivery. Park users will not be restricted from accessing the rest of the park land whilst works are in progress.



Further details of this routing will be found in the attached **Visuals Appendices**

We have already been in contact with Highways about the practicalities of delivering this project and from our most recent conversation with Amanda Pugh and in line with the comments given in the Pre-Application Advice letter from Kayleigh Lazell, we recognise a number of issues during and post the construction phase will need to be agreed.

We spoke with Dean Howard from Highways on 7th October and discussed the requirements of the project and we believe we have a delivery plan that will meet with approval. A copy of the Traffic Management Plan has been forwarded to him to allow him to view it ahead of the formal planning process. The TMP submitted with this application will have incorporated any amendments suggested by Mr Howard as a result of this information exchange.

We believe that a TRO will be placed on the road immediately adjacent to the skatepark once the park has been completed to stop random “drop off’s” of skaters wishing to use the park, but since this is likely to happen after the park is completed, it will not have an impact on the delivery of the project.

Please see:

Appendix 4.0 Mumbles Traffic Management Plan

Access for those arriving by bike

This project is ideally suited to anyone who chooses to arrive by cycle and compliments MCC's commitment to promoting the use of bikes and cycling in Mumbles.

The site sits exactly on the National Cycle Network Route 4 linking many larger towns and cities with the sites location. It is the Promenade Path we have referenced many times which makes up this section of NCNR4.



Existing Services Statement

In line with good operational practices, desktop surveys have already been carried out for the known utility companies who have services in the area.

Returns were received identifying returns on the site.

The location and orientation of the skatepark has been positioned to ensure the owners of the service runs will not object to the provision of the facility by adhering to their easement requirements, nor will it present a danger to the build crew during the construction phase.

A composite overlay showing the known service runs and the skatepark has been provided for your information purposes.

Please See:

Appendix 5.0 Services and Skatepark Composite Overlay

Flood risk assessment and statement

Given the site partially sits on a flood zone, we have instructed the commissioning of a formal Flood Risk Assessment to accompany this application.

To aid the production of this, we were able to provide M-EC Consulting Development Engineers with the Geotechnical report we had commissioned (see following page) and the required 3D visualisations along with a Topographical Survey which was carried out by Dorset Land Surveying Ltd.

We are mindful that the scheme needs SuDS consent and SAB approval as well, the suggested drainage solution will be found within the FRA and appropriate Engineering Drawing and SAB approval will be sought through the appropriate and separate SAB approval channel. M-EC are also undertaking the documentation creation for this application.

This report is attached as

Appendix 6.0 Flood Risk Assessment

Geotechnical Report

To ensure we can provide a fully reasoned and thought out application, a Geotechnical Report has been commissioned and carried out by Terra Firma Ltd to ensure the exact ground conditions are known and percolation tests carried out to shape the drainage solution for the skatepark.

This report is attached as

Appendix 7.0 Geotechnical Report

and

Appendix 7.1 Site Photos Complementing Geo Report

Tree Survey

We have commissioned a Tree Survey, Tree Constraints Plan and summary report – compliant to BS5837 (2012)

and

An Arboricultural Impact Assessment and Tree Protection Plan / Method Statement from Arbtech Consulting Ltd whom we believe are known to Swansea City Council and by default, Swansea City's planning department.

This report is attached as

Appendix 8.0 Tree Survey and Summary Report.

Lighting Strategy

We are aware of the need for a sensitive lighting strategy as outlined in the Pre-Application Advice letter issued 05.06.19.

However, we cannot see the need for a separate document to outline this since our construction methodology does not call for temporary floodlighting during the build process. The site will not be illuminated in any way, at any point of the build procedure, and we would be happy to sign a declaration to this point to satisfy the designated LPA Planning Ecologist or indeed meet with them if this is deemed to be a requirement.

Site work will only take place during daylight hours – typically 7.30am through to about 5.30pm Monday to Friday with no work taking place over the weekend.

Nor, it should be noted, are there any current proposals to flood light the park once completed.

Ecological Assessment

A Preliminary Ecological Assessment (PEA) has been carried out as requested in the Pre-Application Advice letter.

Please see:

Appendix 9.0 Preliminary Ecological Assessment

Pollution Prevention

A Construction Environmental Management Plan has been provided as requested in the Pre-Application Advice letter.

Please see:

Appendix 10.0 Construction Environmental Management Plan

Additional works proposed on the site

Additional planting

Provision has been made to allow for additional planting to both enhance visual protection from outside of the park area when viewed from Mumbles Road. It is suggested that we specify a hardy plant which will grow into a dense hedge line that is also attractive.

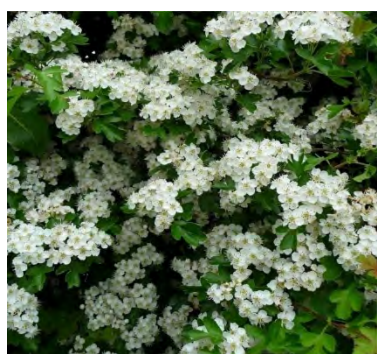
Blackthorn (*Prunus spinosa*) is an ideal plant for maritime conditions, it is a native plant to the UK and specifically the Gower and bears copious prays of white blossom in April and purple black fruits (sloes) in Autumn. The branches have black bark and thorns – which obviously gives the plant its name.



We have had extensive experience in the specification of planting in and around skate parks and we have to be mindful of the typical demographic of the people who will be using the facility. Without wishing to cast aspersions on any individuals, typically, your average skater is inclined to take the shortest route from A to B (A being an approach point from the road or path and B being the skate park), and this will include going through planting schemes. Therefore, we suggest that hedge lines are best suited to species which will discourage this. A dense, hard wooded and thorny plant such as Blackthorn therefore is an ideal candidate and has worked well for us in the past, reaching maturity without skater shaped gaps through the hedges appearing.

Common Hawthorn (*Crataegus monogyna* or perhaps *Rhaphiolepis umbellata*) for example, could provide a logical alternative. It is suitable to all soil types and sites other than dense shade or very wet soils. They are tolerant to exposed weather conditions and form hedgerows beautifully. It is also a native species to the UK.

It provides an abundance of white scented blossom in May and develops glossy red haws in autumn. As with the Blackthorn, branches are also covered with thorns.



Hawthorn hedges can grow to a height of between 1.5 and 4 m with a similar growth rate to the Blackthorn. Once a desired height is reached or the council think the hedge should be trimmed back, pruning for both is best done after flowering or in the autumn.

Mumbles Community Council, however, would be open to suggestion from the planners should there be a preference for a different planting scheme to the one suggested.


Wild flower seeding

The council are also proposing to enhance the land surrounding the skate park (see attached visual) by sowing wild flower seeds that are native to the area. Genus of flowers to be agreed with the relevant department's representative at City of Swansea Council however, we have received advice from Mr Alan Kearsley-Evans of the National Trust who suggested we look at specifying a mix known as Pollen and Nectar Floristically enhanced margin flowers.

Fencing

A short length of fencing has now been specified to comply with the suggestions made by planners at the pre-application advice stage. There will not be a gate in the fence line. The fence line will sit inside the new hedge line as described above, facing into the skate park. The hedges will, in a short space of time, hide this new fence when viewed from Mumbles Road.

The preference will be to specify galvanised, then powder coated bow top fencing, 1.2m in height. The RAL colour is yet to be specified, but it is thought RAL 6037 Pure Green would be a suitably muted colour. If your planners prefer something different, we would be happy to accommodate any other suggestion.

 <p>PURE GREEN</p>	RAL 6024	Verduynrood	Troffs green	Vert signalrood	Verde taffoa	Verde brillon	Verduynrood
	RAL 6025	Faengroen	Fenn green	Vert faugon	Verde Mincho	Verde fene	Verduynrood
	RAL 6026	Heugroen	Heugroen	Vert heug	Verde heug	Verde heug	Verduynrood
	RAL 6027	Lichtgroen	Light green	Vert clair	Verde luminosa	Verde chiaro	Lichtgroen
	RAL 6028	Bakergroen	Pine green	Vert pin	Verde pin	Verde pin	Pinegroen
	RAL 6029	Mazegroen	Leek green	Vert leek	Verde leek	Verde leek	Mazegroen
	RAL 6032	Signalgroen	Signal green	Vert de securite	Verde segnale	Verde segnale	Signalgroen
	RAL 6033	Middelgroen	Mild turquoise	Turquoise mediate	Turquoise mediate	Turchese mediate	Middelgroen
	RAL 6034	Pastelturkis	Pastel turquoise	Turquoise pastel	Turquoise pastel	Turchese pastel	Pastelturquoise
	RAL 6035	Pastelgroen	Pastel green	Vert pastel	Verde pastel	Verde pastel	Pastelgroen
	RAL 6036	Heugroen	Pastel rood groen	Vert rood groen	Verde rood groen	Verde rood groen	Heugroen
	RAL 6037	Heugroen	Pure green	Vert pin	Verde pin	Verde pin	Heugroen
	RAL 6038	Leuchtgroen	Luminous green	Vert brillant	Verde brillante	Verde brillante	Leuchtgroen



Images of the area





Views from within the site



List of Appendices

Appendix 1.0 to 1.7: Location Plans, Isometric, and Dimensioned Visuals of the Wheeled Sport Arena

Appendix 2.0: Dorchester Police Press Release, Dorchester Skate Park and the drop in antisocial behaviour figures

Appendix 2.1: Letter from Inspector L Fry

Appendix 3.0: Skatepark Design Statement

Appendix 4.0: Mumbles Traffic management Plan

Appendix 5.0: Services and Skatepark Composite Overlay

Appendix 6.0: Flood Risk Assessment

Appendix 7.0: Geotechnical Report

Appendix 7.1: Photos Complementing the Geotechnical report

Appendix 8.0: Tree Survey and Summary Report

Appendix 9.0: Preliminary Ecological Assessment

Appendix 10.0: Construction Environmental Management Plan

Design Access Statement written September 2019

Agent representing MCC - Mark Clogg, Director of Maverick Industries Ltd

Llwynderw Skatepark

OFF MUMBLES ROAD

WEST CROSS

SWANSEA

SA3 5AN

(80M FROM SITE)

PRE-CONSTRUCTION PHASE

HEALTH AND SAFETY PLAN

OCTOBER 2018



MAVERICK
INDUSTRIES

REG. OFFICE

UNIT G2 . ARENA BUSINESS CENTRE
HOLYROOD CLOSE . POOLE . BH177FJ

WWW.MAVERICKINDUSTRIES.CO.UK
INFO@MAVERICKINDUSTRIES.CO.UK
TEL 01202 607 475 | FAX 08456529991

COMPANY REG
06202977
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INDUSTRIES

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1. Project Directory
2. Contact Details
3. Project Policy, Organisation and Arrangements
4. Site Consultation and Communication Regarding Health and Safety Hazards
5. Construction Hazards and Control Measures
6. Health and Safety



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UNIT G2 . ARENA BUSINESS CENTRE
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TEL 01202 607 475 | FAX 08456529991

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1. Project Directory

Client	Mumbles Community Council
Telephone	01792 363 589
Email	council@mumbles.co.uk
Contact	Steve Heydon - Clerk
Contract Administrator	As Above
Telephone	
Email	
Contact	
Designers / Principle Contractors	Maverick Industries Ltd
Telephone	01202 607 475
Email	info@maverickindustries.co.uk
Contact	Sue Mitchener / Russ Holbert
H&S Contact	Mark Clogg – Maverick Industries Ltd
Telephone	01202 607473
Site Foreman	Rob Farrow – Maverick Industries Ltd
Telephone	07402 346 864
Safety Advisors	MSAFE Risk Management Ltd
Telephone	01202 470 050
Contact	Nathan Pitt BSc (Hons) CMiOSH MaPS ICIQB
Specialist Sub Contractor (Survey Work)	Dorset Land Surveying Ltd
Telephone	01202 896 481
Contact	Gary Vaughan Civil Engineer. BSc. Civil Engineering, MCIInstCES

2. Project Details

2.1 Scope of Works

The project comprises of the following elements

Element of structure	Construction Method
Foundations and base structure	See Method statement for details
Concrete Ramp Sub Structure	See Method statement for details
Concrete facings	See Method statement for details
Other	See Method statement for details
Coping	See Method statement for details
Handrails	N/A on this project



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2.2 Site Specific Hazards

Hazards associated with the site include:

Work within a residential area adjacent to the public highway and occupied buildings

Work adjacent to recreational areas and children

Significant risk of unauthorised entry to site by trespassers

Summary of hazards associated with the project

Site Set Up	Site security, unauthorised access and use of the site compound and offices, plant machinery and temporary structures
Working at Height	Unloading from Lorries Setting up Ramps Forming Concrete Banks and bunds Working from ladders
Excavations and Foundations	Working close to existing services Deep excavations Working at excavation faces during blinding and re-bar phase
Manual Handling	Re-bar Ramp Components Blocks and Bricks
Traffic management	Adjacent roads may have many cars parked on same limiting visibility and manoeuvres

Hazards associated with the site include:

-Work within a residential area adjacent to the public highway and occupied buildings

-Work adjacent to recreational areas and therefore Children as well

-Significant risk of unauthorised entry to site by trespassers

-Restricted access to site by way of narrow roads, overhanging trees, gates and parked vehicles



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TEL 01202 607 475 | FAX 08456529991

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Photo of the site / Brief description of accessibility issues and obvious hazards

Site address: Llwynderw Skate Park, **off Mumbles Road, Mumbles, SA3 5AN.**

Access and egress from the site will be by the same route. Mumbles Road is very busy and it is imperative that banksmen are positioned to control the traffic in both directions when deliveries take place and when vehicles are exiting the site.

If we are chosen as the preferred contractor we will create a traffic management plan which takes in all known stipulations from Highways Agency as a planning condition - if any are stated.

To avoid damage to the grass a short temporary trackway is needed and is indicated below and on the last page
Routing of delivery vehicles have them driving in and then turning around at the delivery point and driving straight out onto the road with the aid of two banksmen.

Members of the public are bound to be about so as usual we will require to be mindful of curious visitors and use banksmen for access and egress of delivery vehicles which will be restricted to crawling speed. Additionally, all vehicles visiting the site are required to travel at this speed and have hazard warning lights on for the duration of the drive. There is a point where a fall in road height of appx 1 metre appears over a distance of appx 20 to 30 m it is within the angles of acceptability for delivery vehicles. Trackway must not however be laid with excessive sideways angle.

A warning sign for pedestrians will be placed at the entrance of both our site and the access road. Additional warning signs will be placed on the perimeter fence. Site crew will informally direct the public away from site if appropriate.

The site shall be fenced in its entirety with herras fencing as shown on the final page.





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Photos of the route in



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3. Project Policy, Organisation and Arrangements

3.1 Project policy

It is the policy of Maverick on this project that all works shall be carried out with the minimum of risk to the health and safety of all persons employed and any other persons likely to be affected.

This policy covers all work being carried out within the scope and definition of the project and under the management, supervision, direction or control of the company. This will include sub-contractors and suppliers of plant, equipment, materials and labour employed on and for the purpose of the project works.

In order to achieve this objective, all work activities, work systems, plant, equipment and materials shall, so far as is reasonably practicable be planned, maintained and managed to reduce risks to health and safety to site staff and anybody else potential affected by site activities.

3.2 Organisation

It is the responsibility of the Directors to determine the organisation necessary to manage and supervise the works and to ensure that individual responsibilities are defined accordingly. In addition to any specific responsibilities warranted on account of the nature of the work being undertaken, a Site Manager must be appointed with overall responsibility for the supervision and co-ordination of the work.

3.3 Arrangements

The Site Health and Safety Plan has been prepared in order to identify the extent of the work to be carried out and the arrangements necessary to ensure the safe performance of the project. The plan also identifies the hazards involved in the execution of the works and the risk assessments which will need to be carried out in order to determine the control measures necessary to eliminate or minimise those risks.

Method statements and risk assessments must be submitted by all contractors and sub-contractors for checking prior to work commencing. Each contractor is responsible for ensuring the relevant information as required under Regulation 9 of the Management of Health and safety at Work Regulations is passed to those identified or considered to be at risk with copies to site. In carrying out the responsibilities of Principal Contractor, they will co-ordinate and where necessary, manage the arrangements currently in force so as to ensure that there is an integrated approach to Health and Safety on site.

Copies of Method Statements, Risk Assessments and relevant information notices shall be incorporated into a single file maintained on site.

The Site Foreman shall, as necessary review the plan so as to ensure that it is amended and updated to take into account of the changing circumstances of the project.

The plan specifies the arrangements for site induction training and safety awareness talks, which apply to all persons employed on the site including sub-contractor's personnel and site visitors. Full records must be kept of these activities and retained in the site safety folder.

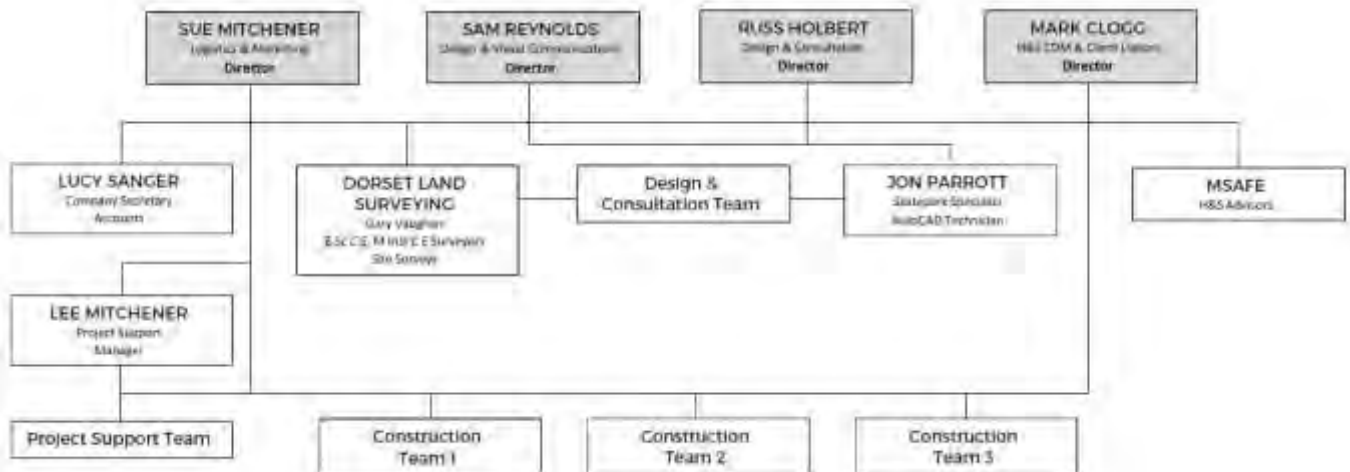


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3.4 Project Organisation



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3.5 Duties of Persons with specific responsibilities

Site Safety Supervisor:	<p>The Site Safety Supervisor will have overall responsibility for the safety, health and welfare, supervision and co-ordination of work.</p> <p>Specific responsibility: Site Foreman</p>
Induction Training:	<p>Induction training will be carried out for all operatives on site. They will be shown the site safety rules, be made aware of the location of the first aid facilities and identity of the site trained first aider. They will also be made aware of the emergency procedures and provision regarding fire safety and escape on site.</p> <p>Specific responsibility: Mark Clogg</p>
Lifting Co-ordinator:	<p>Responsibility for the organisation and control of crane lifting operations and will carry out the following duties:</p> <ul style="list-style-type: none">- Make assessment of all lifting operations.- Ensure adequate inspection / maintenance records of lifting plant and equipment.- Produce "Lifting Plan" and co-ordinate lifting operations by competent persons- Ensure that suitably trained and competent Slinger, Banksman and Crane Driver are provided. <p>Specific responsibility: Site Foreman</p>
Fire Safety Co-ordinator:	<p>It is this person's responsibility to ensure all arrangements set out in the Site Fire Plan are carried out and regular inspections of the site fire equipment recorded.</p> <p>Specific responsibility: Site Foreman</p>
First Aider:	<p>In the event of an accident the first aider will ensure the following:</p> <ul style="list-style-type: none">- Ensure, if required, transport is arranged to take the injured person to hospital.- Advise the injured person to make an appointment with their own doctor.- Ensure all details are entered in the accident book.- Inform the Head Office, if appropriate, to ensure that an accident investigation is undertaken. <p>Specific responsibility: Designated Site First Aider</p>



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3.6 Details of Inspections required on site

Site Safety Inspections	Safety inspections to be carried out as detailed in the Company Safety Policy. Construction Manager: Daily and Weekly
Excavations	To be inspected at the start of each shift and at least once a day while persons are employed therein and thorough examination must be carried out weekly and entered into the Excavation Register. Specific responsibility: Site Foreman
Lifting Appliances	To be inspected before use on site and every 7 days thereafter by a competent person. The inspection will be made in a register held on site and this will be inspected by the Site Manger to ensure that this is kept up to date. Specific responsibility: Excavator operator, Dumper Operator, Crane Operator, Telehandler Operator
Electric	Temporary electrics to be visually checked as part of the site inspections and a competent electrical contractor to carry out maintenance / test of equipment as required and issue certification accordingly. Subcontractors to ensure P.A.T portable appliance testing of their equipment is actioned and recorded. Specific responsibility: Site Foreman
First Aider	Responsible for ensuring the Fist Aid box is stocked and any items are replaced following removal. Specific responsibility: Designated First Aider
Principal Designer	Responsibility for H&S interface between client and contractor and enforcement of HSE directives Specific responsibility: Mark Clogg



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3.7 Specific Responsibilities

Directors	<ul style="list-style-type: none">- Overall implementation and regular review of the Maverick Health and Safety Policy.- Providing sufficient resources for managing health and safety.- Ensuring legislative requirements are complied with.- Ensuring staff receive adequate information on the risks associated with their work, sufficient and suitable equipment, safe place of work, and training for health and safety.- Maverick Directors take responsibility for making adequate provision for health and safety.- Ensuring Site Foremen are aware of their responsibilities.- Ensuring Site Foremen undertake their health and safety duties.- Assessing the competence of contractors.- Coordinating safe systems of work.- Ensuring risk assessments are in place.- Ensuring sufficient and adequate facilities are supplied in relation to welfare and first aid.- Establishing the site rules in accordance with the Health and Safety Plan and Maverick's Health and Safety Policy.
Construction Managers	<ul style="list-style-type: none">- Ensuring adequate resources for the site set up- Ensuring Site Foremen are aware of their responsibilities.- Ensuring Site Foremen undertake their health and safety duties.- Assessing the competence of contractors.- Coordinating safe systems of work.- Ensuring risk assessments are in place.- Ensuring sufficient and adequate facilities are supplied in relation to welfare and first aid.- Establishing the site rules in accordance with the Health and Safety Plan and Maverick's Health and Safety Policy.
Site Foreman	<ul style="list-style-type: none">- Apply the risk control systems and workplace precautions applicable to works on site. Identify any health and safety deficiencies and bring them to the attention of the Directors.- Coordinating health and safety on site.- Ensure all operatives receive a site specific induction and have received the relevant training to ensure their competence.- Ensure all work equipment and PPE is maintained and used properly. Ensure users have received the appropriate training.- Monitor on-site safety and compliance with safety requirements through site inspections.- Report all accidents, occupational ill health and near misses to the Directors immediately and assist in the investigation of the same.- Conducting toolbox talks and safety inductions.- Ensuring safe systems of work are adhered to.- Ensuring that only authorised persons enter site.



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	<ul style="list-style-type: none"> - Ensuring that welfare and first aid facilities provided remain in good order. - Enforcing the site rules in accordance with the Health and Safety Plan and Maverick Safety Policy.
Consultants	<ul style="list-style-type: none"> - The designers are to make adequate provision for health and safety in their design in accordance with CDM 2015 regulations. - The designers are to forward any information in relation to significant health and safety hazards which have not been “designed out” in accordance with CDM 2015 regulations.
Contractors	<ul style="list-style-type: none"> - Use all work equipment and PPE properly and in accordance with training received. - Report any safety deficiencies, accidents, cases of occupational ill health, near misses and horse play to the Site Manager, or if necessary the Directors. - Assist the Principal Contractor during investigations of accidents and safety related incidents. - Work safely at all times giving due consideration for the safety of others. - Notify the Site Foreman of safety training needs. - Assist the Principal Contractor in improving safety systems and performance through consultation, co-operation and communication. - Providing staff with sufficient, suitable and safe equipment to enable them to complete their tasks in a safe manner. - Provide risk assessments, COSHH assessments and method statements (if requested) to the Directors prior to the commencement of their works.



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3.8 Health and safety Goals

- No unauthorised access to site.
- Adequate hazard warning signage throughout the duration of the project.
- Adequate welfare facilities to be maintained throughout the duration of the project.
- No infringements on the site rules, Health and Safety Plan and Maverick' Health and Safety Policy.
- Clear and efficient communication between all parties.
- No major accidents / incidents during the project.
- All incidents and near misses to be recorded.
- High level of public protection to be provided at all times.
- No recurrence of breaches in safety legislation on subsequent safety inspections reports.

3.9 Site security

It is the Site Foreman's responsibility to ensure that the site is left in a safe condition at the end of each working day. However, we would ask all contractors to assist in this responsibility by re-clipping any heras fencing and reporting any areas where there is easy access to the site.

Where practicable excavations should be backfilled or barriered to enable any trespassers to see their location in poor light.

Ladders to scaffolding should be boarded or removed to restrict access.

All equipment to be left inoperable at the end of each working day

A final check of all the site perimeter fencing is to be made by the Site Foreman or his allocated member of staff prior to the site entrance being locked. This final check should also ensure the safe storage of materials which could become unstable during the night due to vandalism or adverse weather conditions.

3.10 Monitoring and Review of Site Safety Performance

The Site Foreman will be responsible for the management of health and safety on site. Any major incidents or issues that they have not been able to rectify will be reported to the Directors for further action. The Directors shall also visit site regularly and report findings to the Site Foreman.

Formal safety reviews will be held at defined intervals with the safety reports forming an integral part of the same. The review will serve to identify any trends and recurring beaches in Health and Safety legislation / best practice and will endeavour to ascertain what further control measures or training will be required to address the same.

The review will also serve to identify how sub-contractors have performed during the project and whether they should be re-appointed for further works.

The Site Foreman will be responsible for the management of health and safety on site. Any major incidents or issues that they have not been able to rectify will be reported to the Directors for further action. The Directors shall also visit site regularly and report findings Site consultation and communication regarding Health and Safety

4. Site Consultation and communication regarding Health and Safety

4.1 Project Team

Progress meetings will be held during the course of the project with health, safety and welfare matters including those specific to CDM and design forming an important part of the agenda. These meetings will be attended by the Principal Contractor, Principal Designers and the Client and / or the Client's Representative.

Contractors / Employees

Works contractors are required to both provide any relevant information pertaining to health and safety at weekly meetings with the Principal Contactor and communicated information provided for them to their employees with regard to safety measures discussed.

Regular site safety meetings will be held by the Site Foreman and attended by representatives of site operatives and sub-contractors. During these meetings, relevant information pertaining to Risk Assessments and Safe Systems of Work will be discussed and future training requirements will be identified specific to the works.

Communication / Co-operation

Health and Safety matters will be discussed as part of the agenda at the regular Project Team meetings. The meetings will be attended by the Client and / or the Client's representative, Designers and the Principal Contactor.

These have been scheduled to take place fortnightly with site foreman and monthly with Maverick Management representative.

Continued liaison will include review of the Health and Safety Plan following any substantial design changes that may have potential Health and Safety implications. Consideration must be given to "design out" or at least minimise the risks associated with the same and introduce control measures in compliance with the CDM 2015 Regulations and good working practice.

The Principal Designer and where applicable any works contactors, must reassess the Health and Safety implications of any substantial design changes and introduce control measures accordingly.



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4.2 Continued Liaison: Principal Designers

Any substantial design changes shall be examined by the Designer(s) for Health and safety implications, hazards identified, risks assessed and consideration given to avoiding, minimising or controlling risks in accordance with CDM 2015 Regulations and the Approved Code of Practice.

Details of proposed substantial design changes must be submitted to the Principal Designer to ensure compliance with the Regulations.

The Principal Contractor shall re-examine the Health and Safety implications of any design changes and implement all necessary measures to mitigate any risks associated with the same.

Non – design generated work / site developments (which had not or could not have been envisaged) necessitating a revised approach, must be brought to the attention of and evaluated by the Designer(s).

The Designer's resolution of any such unforeseen circumstances

4.3 Selection and Control of Subcontractors

All subcontractors will be assessed for competence with specific reference to Health and Safety prior to their appointment.

All sub-contractors will complete an assessment questionnaire prior to subcontract orders being placed. Orders will only proceed with contractors who satisfy all Health and Safety criteria applicable to the works.

4.4 Site Induction

All employees, sub-contractors and other visitors to site will receive a verbal induction prior to entering the workplace.

As part of the induction, all operatives will be made aware of the risk assessments and safe systems of work associated with their activities on site. Operatives will be required to read these documents and sign a copy of the induction form as confirm that they have read and understood the content of the same.

Inductions will include:

A	Senior management commitment to health and safety
B	The outline of the project
C	The individual's immediate line manager and any other key personnel
D	Any site-specific health and safety risks, for example in relation to access, transport, site Contamination, hazardous substances and manual handling
E	Control measures on the site, including: Any site rules; Any permit-to-work systems; Traffic routes; Security arrangements; Hearing protection zones



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F	Arrangements for personal protective equipment, including what is needed, where to find it and how to use it
G	Arrangements for housekeeping and materials storage
H	Facilities available, including welfare facilities
I	Emergency procedures, including fire precautions, the action to take in the event of a fire, Escape routes, assembly points, responsible people and the safe use of any firefighting Equipment
J	Arrangements for first aid
K	Arrangements for reporting accidents and other incidents
L	Details of any planned training, such as 'toolbox' talks
M	Arrangements for consulting and involving workers in health and safety, including the Identity and role of any
N	Information about the individual's responsibilities for health and safety

4.5 Reporting and Investigation of accidents and incidents including near misses

Should an incident occur, the Directors aided by the Safety Adviser shall investigate the incident and produce an investigation report, enter the details in the accident record book no matter how trivial they may seem and complete HSE form F2508 (rev1/86) for those incidents that are reportable to the HSE.

It is the duty of all contractors to report all accidents and near misses, no matter how trivial they may seem to the Site Foreman. A near miss may result in serious injury or worse, next time it occurs. Therefore measures can be taken to prevent a near miss from reoccurring if they are reported to the Site Foreman. A copy of our reporting procedures policy can be obtained upon request.

4.6 Welfare Facilities

The following facilities will be provided;

Running hot water

Resting / dining area with table and benches with backs

Toilet facilities within groundhog unit

Paper towels, Soap, Cups

The above will be made available in the temporary facilities situated within the site boundary. This facility will incorporate Site Foreman's office and mess hut.



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All contractors have a responsibility to respect the facilities provided, ensuring that the same are kept reasonably tidy and clean throughout the duration of the contract. Contractors are asked to inform Maverick if any persons are not adhering to this rule, or if any supplies of soap, water, paper towels or toilet paper have depleted. Smoking within any cabin where there are non-smokers is prohibited.

It is the Site Foreman's responsibility or his allocated member of staff to ensure that adequate supplies are maintained, and the facilities are kept clean and tidy throughout the construction phase.

4.7 First Aid

First aid will be administered by the appointed first-aider, (To be confirmed). The Site Foreman is responsible for ensuring that adequate and sufficient first aid facilities are available in the site office for the duration of the contract.

All operatives shall be made aware of the identity of the trained first-aider on site and where the first-aid facilities are kept. Suitable signs will be erected in the location of the first-aid facilities, detailing the same.

The nearest accident and emergency unit is; **8.4miles away (30 mins by car) 01792 702222**

Morrison Hospital
Heol Maes Eglwys
Morrison
Swansea
SA6 6NL

4.8 Fire and Emergency Arrangements

On discovery of a fire on site, the following action must be undertaken:

Shout "FIRE, FIRE, FIRE."

Attack the fire if possible with equipment provided in the site hut, but do not take any personal risks.

On hearing the alarm being raised, all operatives must immediately leave their work areas and head for the muster point.

The Site Foreman will call fire brigade immediately by telephone:

Dial 999.

Give operator the telephone number and ask for FIRE.



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When fire brigade replies give message distinctly:

“FIRE AT: Mumbles / Llwynderw Skate Park, off Mumbles Road, Mumbles, SA3 5AN”

Do not replace receiver until the fire brigade have repeated the address.

Report to the Site Foreman at the muster point, situated on the pathway away from any access points onto the site.

Report any missing persons to the Site Foreman.

The Site Foreman is to confirm whether there are any missing persons using the site diary and signing in book. This information can save lives, as fire officers' lives have been claimed by dangerous rescue attempts when the building on fire has been empty.

4.9 Site Safety Rules

- Pedestrian access to the site is via the vehicular access point OR alternative as shown on schematic below
- No alcohol may be consumed during working hours including overtime.
- No person will be permitted to start work on site if through the consumption of alcohol, their own safety is impaired or the safety of others who may be affected by their actions.
- Hard hats must be worn unless works undertaken are designated as low risk by the Site Foreman.
- Personal protective equipment must be worn by all operatives as prescribed as part of the site risk assessment performed by the Principal Contractor.
- All accidents must be reported to the Site Foreman.
- Only 110 volt electrical equipment may be used unless authorised by the Site Foreman.
- Damaged or faulty equipment must not be used.
- Abrasive wheels must only be operated by competent persons.
- Waste materials must be taken to designated areas on a daily basis.
- Smoking is not permitted on site except in designated areas.
- Only authorised persons are permitted to operate plant and machinery.
- Care is to be taken during hot weather to avoid over exposure to the sun.
- All site operatives must attend an induction course prior to commencing work.
- No work to be commenced on site without the submission and approval of appropriate Risk Assessment / Method Statement.



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4.10 Statement Register and Risk Assessment Matrix

Risk assessments and method statements from sub-contractors will be requested by Maverick and must be received in good time for review by the Site Foreman. They may also request additional assistance and comment from MSAFE to ensure that the same is sufficient and makes suitable provisions to allow the works to proceed without undue risk to Health and Safety.

Method statements are all contained within the on-site H&S file

Design Element	Risk Assessment required: Yes / No	Method Statement required: Yes / No	Lifting Plan required Yes / No	Sign / Date
Ground Works	Yes	Yes	No	CURRENT
Forming concrete	Yes	Yes	No	CURRENT



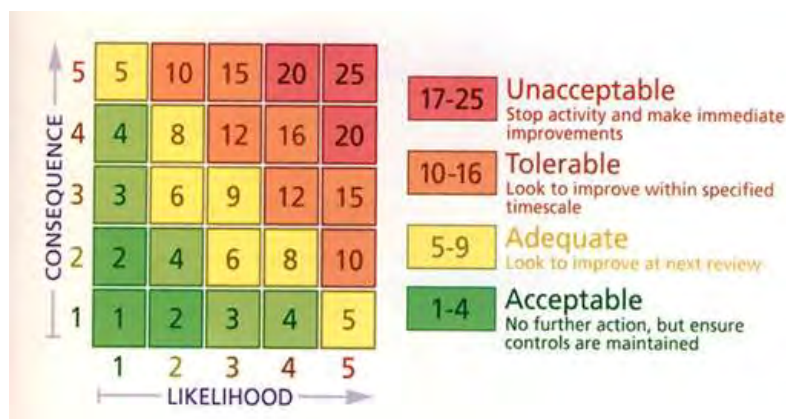
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RISK MATRIX FOR USE IN SECTION 5



<p>The Likelihood or Probability of a hazardous event occurring could be ranked as:</p>	<ol style="list-style-type: none"> 1) Very Unlikely: there's a 1 in a million chance of the hazardous event happening 2) Unlikely: there's a 1 in 100,000 chance of the hazardous event happening 3) Fairly Likely: There's a 1 in 10,000 chance of the hazardous event happening 4) Likely: there's a 1 in 1,000 chance of the hazardous event happening 5) Very Likely: There's a 1 in 100 chance of the hazardous event happening
<p>Consequence would be ranked as:</p>	<ol style="list-style-type: none"> 1) Insignificant: No injury 2) Minor: minor injuries needing first aid 3) Moderate: Up to three days' absence 4) Major: More than three days' absence 5) Catastrophic: Death

The scores shown in the relevant sections indicate the final score after consideration to control measures have been implemented where ever possible or indeed if relevant.

The figures arrived at are by multiplying likelihood x consequence

5. Construction Hazards and Control measures

5.1 Site Set up and Security

RISKS AND HAZARDS ASSOCIATED WITH THE TASK				
<ul style="list-style-type: none"> - Unauthorised entry to site - Fire, vandalism and theft - Injury / death of trespasser - Unauthorised operation of plant and equipment 				
CONTROL MEASURES				
<ul style="list-style-type: none"> - All boundaries where existing boundaries are not adequate are to be protected by 2m high anti climb heras fencing. - Suitable hard standing will be maintained across the site entrance to protect against overloading from heavy plant if deemed necessary by Maverick. - Suitable warning signs to be erected on all obvious site boundaries, warning of the dangers associated with the site and prohibiting any unauthorised access. - Any evidence to suggest unauthorised access during the previous evening is to be reported to the Site Foreman who is to endeavour to improve site security as necessary. - Site Foreman to check perimeter fencing regularly (at least daily) and ensure that the site is secure. - All plant and equipment shall be left immobilised when unattended, especially overnight. All keys to be removed from mobile plant and locked securely in the site office overnight. - Site Foreman to undertake formal inspections and maintenance of public highway adjacent site entrance to ensure that damage or migration of debris does not pose a risk of slips, trips and falls to members of the public. 				
SCORE: LIKELIHOOD 2 X CONSEQUENCE 2 = 4				

5.2 Excavation

RISKS AND HAZARDS ASSOCIATED WITH THE TASK				
<ul style="list-style-type: none"> - Injury/death due to collapse of the excavation whilst in use. - People, materials, vehicles falling into the excavation. - Contact with existing services. - Undermining adjacent structures and uncontrolled subsidence. 				
CONTROL MEASURES				
<ul style="list-style-type: none"> - Excavations are to be battered back to the correct angle of repose where practicable, and backfilled at the earliest possible convenience. No operatives will enter any excavation until the competent supervisor deems it safe for entry. - All significant excavations where entry is required will be inspected at the start of every work shift. - No works shall commence until existing services have been detected. - Any excavation left open for a significant duration will be fenced around or demarcated with high visibility tape. - Any excavation where the depth is assessed as posing a significant risk of falls to operatives or public will have a physical barrier erected around the head of the same. - Spoil removed from trench style excavations where battering has not proved practicable must be kept at least 1m from the leading edge around the top or as far back as the trench is deep. 				
SCORE: LIKELIHOOD 1 X CONSEQUENCE 4 = 4				



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5.3 Removal / Disposal of Waste

RISKS AND HAZARDS ASSOCIATED WITH THE TASK				
<ul style="list-style-type: none">- Build-up of combustible materials.- Contamination.				
CONTROL MEASURES				
<ul style="list-style-type: none">- Practise good housekeeping and have combustible waste taken off site on a regular basis. Keep waste away from sources of ignition.- Practice good hygiene and wear appropriate personal protective equipment.- Adequate welfare facilities will be provided.				
SCORE: LIKELIHOOD 1 X CONSEQUENCE 2 = 2				

5.4 Traffic Management

RISKS AND HAZARDS ASSOCIATED WITH THE TASK				
<ul style="list-style-type: none">- Collision with pedestrians other vehicles on / off site.- Space constraints on site preventing large plant and delivery vehicles from turning on site.- Accessing / egressing the site with a poor visibility.- Soft/ uneven ground conditions.- Entering exiting the site along a narrow side road.				
CONTROL MEASURES				
<ul style="list-style-type: none">- Utilise Banksmen to assist vehicles entering or exiting the site.- temporary trackway system is to be installed- Due to space restrictions on site, it is not possible to adopt a one-way traffic system. Pedestrian access can be via a separate route which is shown on the plan below. Materials will be stored at a designated location guided by site foreman- Banksmen will be highlighted and easily distinguished from other operatives on site by the wearing of high – viz, orange vests. Drivers will be made familiar with the hand signals that the Banksmen intends to use when directing vehicle movement on site. For the safety of the Banksmen, drivers will be made aware that they must keep Banksmen within their field of vision at all times, particularly during reversing. Drivers will be made aware that they must cease all manoeuvring when they lose sight of the Banksmen at any time. Manoeuvring can only continue when the driver regains visual contact with the Banksmen. Banksmen will also be required where vehicles are parked or working on the approach roads. There could be members of public walking adjacent to our site at any time so we need to be on the look- out for them constantly. There is a walking pace speed restriction for site access vehicles.				
SCORE: LIKELIHOOD 2 X CONSEQUENCE 2 = 4				



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5.5 Working at Height

RISKS AND HAZARDS ASSOCIATED WITH THE TASK				
<ul style="list-style-type: none">- Falls from working platforms.- Fall from edges of excavations- Falls off ladders.- Falls of materials.				
CONTROL MEASURES				
<p>- Ladders to be checked for condition on a regular basis to ensure stability and strength. Ladders should be tied at the top or if impracticable anchored at the base to prevent swaying in any direction. Ladders should be placed on a firm level footing and set up at an angle of 75° one out for every four up. The vertical run of a ladder should not be in excess of nine metres without the provision of a landing platform. Ladders should only be used for access purposes only and not as a working platform.</p> <p>- Suitable working platforms with edge protection will be constructed to allow operatives safe access and fall protection during the construction process.</p> <p>- Provide fall protection systems below the working area where there is a danger of falling 2m or more. To be assessed on a site specific basis, but decisions should be made on the following order of preference:</p> <ol style="list-style-type: none">1 – Fully guarded working platforms e.g. correctly installed scaffolding.2 – Engineering fall arrest systems e.g. safety nets and airbags.3 – PPE Fall arrest and fall restraint systems (only to be used as a last resort or in addition to the above methods.)				
SCORE:	LIKELIHOOD 2	X	CONSEQUENCE 2	= 4

5.6 Lifting and Installation of Components

RISKS AND HAZARDS ASSOCIATED WITH THE TASK				
<ul style="list-style-type: none">- Deliveries unloading and storage areas- Materials falling.- Silica dust from scabbling operations.- Unstable ground conditions.- Mishandling of loads in excess of 1 tonne.- Working at height, detailing how operatives will be protected from falls during erection with particular regard to leading edge.				
CONTROL MEASURES				
<p>- A site-specific lifting plan is to be agreed with the Contractor prior to the commencement of works. Ensure the load is correctly slung and that the crane safe working load is adhered to.</p> <p>- The crane should not be permitted to site until suitable hard standing has been laid and it can be ensured that no damage to the underground services can occur. The crane shall enter the site under the co-ordination of a trained Banksman and be located in the occasional / pedestrian site entrance.</p> <p>- All necessary investigations will be undertaken to ensure the suitability of the ground for the type of crane proposed prior to the commencement of works on site. A detailed lifting plan will be developed detailing safe slewing areas.</p> <p>- All lifting operations directed by a trained signaller. All lifting operations to be conducted by a competent crane operator.</p> <p>- All plant and equipment is to be inspected by the Site Foreman prior to the commencement works, and maintenance, testing and inspection records to be provided by the contractor. The lifting equipment must have been tested within the 6 months previous to its use.</p> <p>- Soft landing bags or a suitable alternative will be provided beneath all working areas where there are leading edges or gaps in the working platform to reduce the risk of injuries sustained from falls from height.</p>				
SCORE:	LIKELIHOOD 1	X	CONSEQUENCE 3	= 3



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5.7 Fire

RISKS AND HAZARDS ASSOCIATED WITH THE TASK				
<ul style="list-style-type: none">- Build-up of flammable materials.- Hot works.				
CONTROL MEASURES				
<ul style="list-style-type: none">- Site to be cleared regularly of combustible materials and waste.- Suitable fire points to be installed on the site. These should be placed in the easily accessible locations should consist of 7kg litre water and a 7kg powder extinguisher.				
SCORE: LIKELIHOOD 1 X CONSEQUENCE 2 = 2				

6. Health and safety File

Contractors have a duty to provide the Principal Contractor with any information relating to their works, materials and installations for inclusion in the Health and Safety File.

This information will be collated and filed with Maverick to refer to if needed.



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FIRE ACTION
If You Discover a Fire

Operate the nearest call point, hand bell or claxon

Call the fire brigade by telephoning 999

**If it is safe to fight the fire, do so
Otherwise leave the area by the nearest exit**

**Report to the assembly point on the access road
adjacent the site entrance**

Do not stop to collect personal belongings

ACCIDENT AND EMERGENCY CONTACTS

IN THE EVENT OF AN ACCIDENT OR EMERGENCY

Contact the relevant emergency services on 999

Contact the site first-aider

Send someone to help emergency services locate the accident or emergency

Ensure you and others are not in further danger

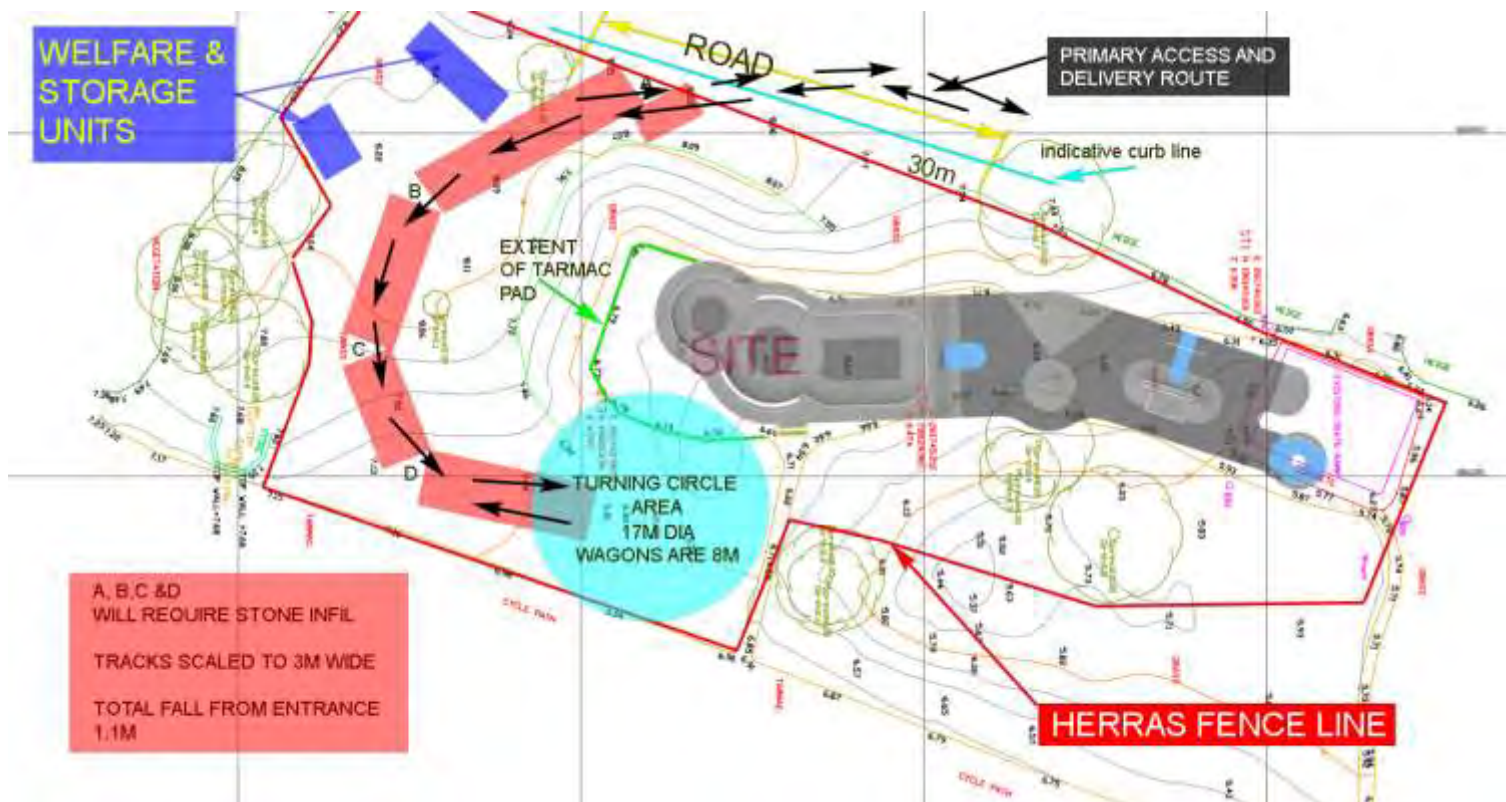
Emergency Contact Numbers

Director:	01202 607 475
Site Foreman: (TBC -Rob Farrow)	07402 346 864
Head Office:	01202 607 475
Nearest Hospital:	01792 702222
Police Station:	101 non emergency



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SITE SET UP PLAN



Created by Mark Clogg

Director - Maverick Industries Ltd - Health & Safety Officer

23rd October 2018



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CONCRETE CONSTRUCTION STATEMENT MAVERICK INDUSTRIES LTD

PART 1: CONCRETE SPECIFICATION

Compliance Notes

Cement specification

Cement used will be ordinary and rapid hardening Portland cement compliant with EN197 or alternatively with the national standard on regulations valid in the place of use of the sprayed concrete.

Aggregates

Aggregate to comply with BS882 and shall be appropriate to the requirements of the application. For dry mixes the natural moisture content in the aggregate

Sand

Sands sourced will be cleaned and washed and compliant with the requirements of BS882

Water

Mixing water shall comply with the requirements of EN1008

Admixture

Admixtures for concrete and sprayed concrete shall comply with the requirements of EN934-2 and EN934-5 respectively and sprayed concrete admixtures with appendix 1. Other admixtures shall be permitted provided that they comply with the general requirements of industry standards Full account should be taken of their effect on the finished sprayed concrete and there should be sufficient data about their suitability, including previous experience with such materials.

Steel Reinforcement

Reinforcements specified shall comply with the requirements of EN206–1 and EN10080

Concrete

This shall comply to the requirements of EN206 – Concrete strength Specification shall conform to the following criteria as a minimum

Foundations: C16 / C20
Riding Surfaces: 28 day strength 40.2 N/mm² (mean average)

Additions (if we chose to specify them)

The most commonly used additions in sprayed concrete are fly ash, ground granulated blast furnace slag and silica fume. These additions shall comply with the relevant European standards or regulations in place in the use of sprayed concrete such as EN450.

Curing agents (if we chose to specify them)

The curing agents shall be in accordance with the relevant European Standards or the National standards valid of the use of sprayed concrete

General Requirements

Consistency

The consistency required for wet spraying depends on the type of conveyance and the application procedure. For a given cement content & w/c ratio the consistency can be adjusted by admixtures added at the mixing plant or on site. Typically speaking we will be specifying a P450 mix with a 70 slump.

Working Temperature

The temperature of the mix before placing shall not be below 2°C and shall not exceed 35°C unless special provisions are in place. Spraying shall not be undertaken when ambient temperature is below 2°C or 2°C and falling.

Requirements of Durability – General

Maverick will produce a durable concrete which protects the reinforcing steel against corrosion, and withstands satisfactorily the working and environmental conditions to which it is exposed during its lifetime. The following factors shall be taken into consideration:

- Choice of suitable constituents, containing no harmful components, or those which may interact unfavourably.
- Choose a concrete composition that will satisfy all specified performance criteria.
- Chloride contents in in-situ sprayed concrete shall not exceed the values that are specified in ENV206 section 5.5
- Alkali content shall comply to the requirements of current National Standards to prevent Alkali – Silica reaction.

Method of Construction Preparatory work

Before spraying of the concrete can start the following preparatory work must be done:

- Poor and loose rocks or stones shall be removed from the surface
- Water ingress shall be engineered out by drainage solutions in line with our engineering drawing specification for the site.
- Loose soil shall be compacted and stabilised.
- Levels shall be set, coping bars placed in their final position and appropriate drainage systems constructed and tested.
- Appropriate rebar networks shall be installed on transitional elements and metal rib systems set utilising screed rails or similar.



Execution of Spraying

For spraying, the following shall be undertaken

- Preparatory wetting shall be carried out unless otherwise specified
- Larger cavities shall be carefully built up before the main application.
- Spraying shall start from the bottom and continue upwards (this is to avoid spraying in rebound losses).
- The nozzle direction shall generally be maintained at 90° to the surface
- Velocity and spraying distance shall be the optimum for maximum concrete adherence and compaction.

Curing

Sprayed concrete shall be cured in accordance with the recommendations set out in EN206 or any other method which has been shown to allow continuous hydration of cement over the curing period.

Curing agents which may impair bonding shall not be used where a further layer of sprayed concrete is to be applied.

Protection against frost is needed until the sprayed concrete has cured to the satisfaction of the Maverick site supervisor.

Our preferred method of curing however is to allow the concrete to cure naturally over time in controlled conditions without recourse to acceleration or retarding agents. It is anticipated that this time will be in the region of 4 to 6 hours after pouring

The Installation Process

Excavation

Check levels and validate throughout process

- Partially excavate 6m section of the site to the required depth (as per design)
- Using an excavator, excavate/grade ground in line with the profile of the previously installed rib sections
- Set in a new section of floor runner/shuttering, pin into ground and set in starter bars as specified
- Fill 6m section of excavated ground with 70mm of lean mix allowing for the removal of the shuttering and cross bracing
- Lay reinforcement mesh onto 75mm hy-chairs

Shotcrete application on Transitions and Flatbank sections

Final shotcrete layer of concrete applied during a dry spell of weather and should be applied to large sections of the skatepark

- Attach pre-rolled sections of coping to inset rib structures
- Spray one 70mm layer of shotcrete, to agreed specification. The layer of shotcrete is designed to encase the reinforcement mesh and provide approximately 40mm to top surface.
- Experienced trowellers will finish the shotcrete surface to an agreed transition surface tolerance of approx 3-4mm.
- Shotcrete layer will also be designed to meet with the coping (Galvanised steel tube 60.3mm OD). Beading will be formed around the top and bottom of the coping to prevent cracking of the concrete. (see Page 8 for details and pictures)



Flat Base/floors

After all transitional and banked sections of the park are completed the flat base will be constructed

- The base will be connected to the transition sections through the starter bars.
- Reinforcement mesh will be laid in the base using the conventional method and will be suspended using plastic or wire Hy-Chairs. The base will be filled with 70mm of lean mix 70mm of shotcrete. The reinforcement mesh will be designed to sit in the shotcrete mix at the same depth, as specified above.

Quality Control

The production of sprayed concrete shall be subject to control procedures. Quality Control is defined as a combination of actions and decisions taken in compliance with specifications and checks to ensure that the specified requirements are satisfied. However since any "P mix" concrete will not be guaranteed by any batching plant it is our experience that dictates conformity. If we decide its wrong, it is simply rejected.

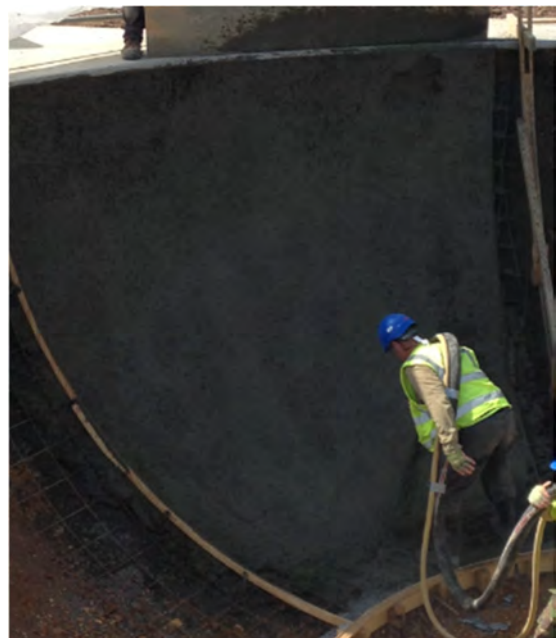
Alignment control

Alignment control is necessary to establish line and grade in sprayed concrete construction and to ensure that proper and uniform material thickness and cover are maintained. Maverick will employ a mixture of depth gauges guide wires and cut ribs to create the formwork needed to ensure the correct angles are maintained especially in the transitional elements of the bowls we are creating.

Surface finishes

The specific finish requirements shall be defined as follows:

"As Shot" The sprayed concrete as left from the nozzle. The sprayed concrete shall be trimmed to true lines using a trammel between two temporary radius shutters (if no fixed radius points exist) and shaped to a uniform finish and left to part cure. When the site foreman decides the concrete is ready, the trowel finishing shall take place.



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Trowelled finishes

Following the treatments as detailed in the “As shot” method the material shall be trowelled smooth in one or more “wipes” depending on the specified levels of polish with one or more of the following:

- Steel Float
- Wooden Float
- Brush
- Sponge



Build up sections

On occasion and where conditions dictate, it may be necessary to build a up a skatepark as a whole, or sections thereof by building up from the ground rather than digging into it or utilising the natural topography of the site. Where this is the case the following methodology will apply.

Retaining walls

Depending on the height of any retaining wall required, Maverick will either seek professional advice from accredited structural engineers and their recommendations will be followed over the composition, width and structural integrity. Or we will adhere to industry standards of low height retaining walls. BS 8006: 1995 governs the specification. Typically speaking we anticipated to utilise one of the following (or similar) to create any wall required.

- Aircrete Blocks 620mm x 215mm x 100mm
- Dense concrete Blocks 3.5n 440mm x 215mm x 100mm
- Hollow Dense Concrete Block 3.5n 440mm x 215mm x 140mm

The finish will conform to any specification laid down by tender documentation, expert advice or Maverick requirements and will be adhered to. Rendering or Spray concrete application will follow in the manner previously described, or alternatively the backfilling of and compacting of soil as required.

Block work

Street sections in Skatepark provision will require blocks or pads to be created with no set dimensional parameters. In addition to:

- Aircrete Blocks 620mm x 215mm x 100mm
- Dense concrete Blocks 3.5n 440mm x 215mm x 100mm
- Hollow Dense Concrete Block 3.5n 440mm x 215mm x 140mm

We can also utilise Polystyrene blocks or simply use wooden box formers. The choice will ultimately be Mavericks and will reflect the needs and strengths of the unit specified. Rendering or Spray concrete application will then follow to create the finished article.



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It is possible that natural topography or compacted subsoil can be used as the base of any works. Spray concrete application in this manner was covered in preceding chapters.

Environment, Health and Safety

A reduction of the environmental impact and improved occupational health and safety has been among the priority objectives in the further development of sprayed concrete technology. Sprayed concrete with alkali-free accelerating admixtures can offer considerable advantages in terms of both environmental protection and occupational health and safety.

The application of sprayed concrete should meet all health, safety and environment regulations valid at the place of use. Prior to commencement of any contract a full risk assessment and safety plan should be established and approved.

Safety of Personnel - Dust concentration

During spraying, the building-site crew is at risk due to dust formation and the pollution of the air. The aerosols formed during the sprayed concrete application may constitute a health hazard and therefore have to be minimised. Dust is classified as an aerosol, as are smoke and mist.

Depending on the location and the point in time, the concentration of dust during spraying is subject to major fluctuations, which have to be considered in the interpretation of the results obtained. The fine dust concentration is assessed on the basis of the maximum permissible concentration of contaminants at the work place measured according to the Austrian Guidelines for Sprayed Concrete, Section 12.5.3. Fine dust is defined as dust likely to penetrate into the alveoli of the lungs.

The maximum permissible contaminant concentration at the work place is equal to the maximum permissible concentration of dust which, in general, does not adversely affect the health of workers in the case of repeated and long-term exposure, usually for eight hours, but for no more than 40 hours a week, without use of personal protective equipment (fine-dust masks).

Permissible dust concentration based on maximum permissible contaminant concentration at the work place:

Quartz content MAC value Type of dust Nature of dust % by weight c[mg/m³]

Q < 1 6 fine dust inert

1 < Q < 3.75 4 fine dust siliceous

Q > 3.75 0.15 fine quartz dust siliceous

(Ref: Austrian Guidelines for Sprayed Concrete, Section 12.5.3)

Given the fact that a range of activities are performed during tunnelling which produce varying amounts of dust, the assessment should be based on the entire working cycle. During spraying, a fine dust concentration of less than twice the relevant maximum permissible contaminant concentration at the work place should be aimed at as an hourly average. If personal protective equipment is used, higher limits are permissible, depending on the protective effect of the equipment. To diminish the dust load over the entire working cycle, the following measures are recommended:

- Dry spraying: use of moist aggregates, machine enclosure, favourable nozzle design, nozzle distance, water content
- Wet spraying with alkali-free accelerators
- Mechanical spraying arms



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- Sufficient ventilation

Health hazards for building-site personnel, above all the risk of skin and eye lesions, can be prevented through the elimination of highly alkaline and strongly irritating admixtures, such as aluminate based products or waterglass.

Personal protection

Personal protection equipment should be always used:

- Helmet
- Goggles, visor
- Dust mask (respirator type - when required, depending on application method and conditions)
- Overalls
- Gloves
- Ear protectors
- Reinforced toe-caps

When applying sprayed concrete overhead, it is not allowed to walk below freshly placed concrete until sufficient strength has been reached. The required time span has to be based on early strength measurements and local conditions (temperature, cement type, dosage/type of sprayed concrete accelerator).

Precautions in the event of blockages of material lines and nozzle

- Whenever a blockage occurs, the operation of the following equipment has to be interrupted:
- Main air supply: to be turned off
- Spraying machine: to be exhausted and/or shut off
- Accelerator pump: to be shut off
- Air supply to nozzle: to be shut off
- Wet spraying machine: take off concrete pressure by reversing the pump.
- Before demounting the line: secure the material lines/nozzle from uncontrolled recoil.
- No personnel in front of the hose under demounting or until the pressure in the material line is relieved.

Safety of hoses and couplings

- Only special reinforced and approved concrete hoses and couplings should be used. In general they should be approved to a bursting pressure equal to twice the actual working pressure.
- All connections/couplings (of concrete, water, accelerator and air hoses) should be equipped with secondary safety fittings.
- All couplings/hoses should be regularly checked and tested.

Environmental issues

Local regulations and standards for environmental issues shall be implemented and followed. The following environmental impacts should be considered:

Impact on soil

In the course of spraying, some of the concrete mix drops to the ground as rebound and is removed together with the excavated material.

Given the fact that the rebound mixes with the excavated material and an environmental impact cannot be altogether excluded, a reduction of the rebound ratio is desirable.



Impact on Water

When used in tunnelling, sprayed concrete may be in contact with rock and ground water. Increased leachability of sprayed concrete may therefore lead to segregation and long-term impact on draining water. Since the leachability of normal concrete is very low even after a short period of hardening, an adverse impact on water quality has not been observed. Thus, concrete qualifies as an environmentally safe construction material. The same applies to sprayed concrete with alkali free accelerators. The use of accelerators based on alkali aluminate and/or silicates increases the portion of leachable materials in sprayed concrete. The leachability of the rebound is also adversely affected.

Although the likelihood of Maverick getting involved with any form of tunnelling work is remote the section above has been included purely as a precautionary measure.

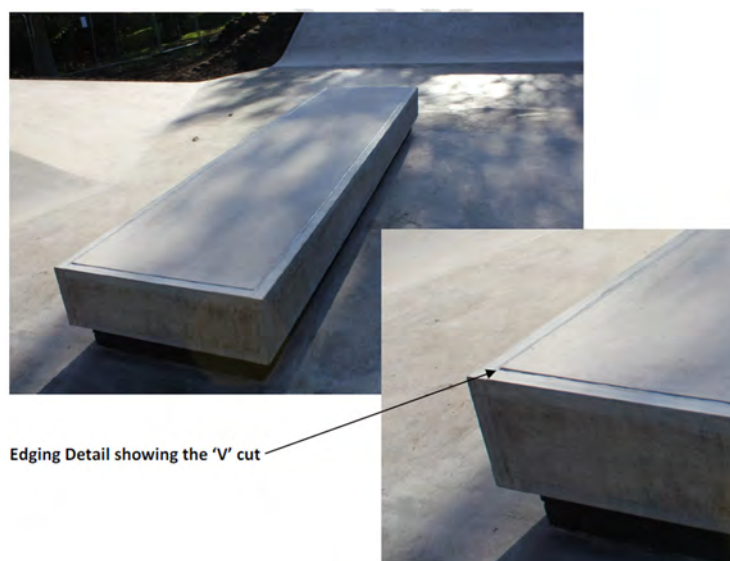
PART 2: STEEL SPECIFICATION

Maverick Industries Ltd were instrumental in creating a sustainable method of construction that will enhance build accuracy and design out the potential for cracking and chipping of concrete elements during the skate park's life time – Please refer to our 'Steel Construction DWGs for detailed specification.

A key element of riding a skate park is the act of 'Grinding'. This is where the rider slides along an edge of a block, side of an element or a line of coping. Conventionally built concrete parks were prone to chipping where the concrete was not protected

During the engineering design phase the scheme is scrutinised to identify sections of the park where skaters will attempt to 'grind'. These areas will then have a variety of galvanised steel edgings specified, ranging from 50mm Box Section or Equal Angle – typically used on Grind Boxes through to rounded beading or strip that will form the convergence line of a pyramid or rollout from a hubba.

By specifying a steel set into a concrete feature, we also need to be mindful of the two differing coefficient rates of linear expansion and thinning of concrete as the two elements join. To combat these facts and remove the chance of any cracking caused by this arrangement we utilise an edging tool that creates a small "V" shape between the steel and the concrete. This can be seen in the two pictures below:



Edging Detail showing the 'V' cut



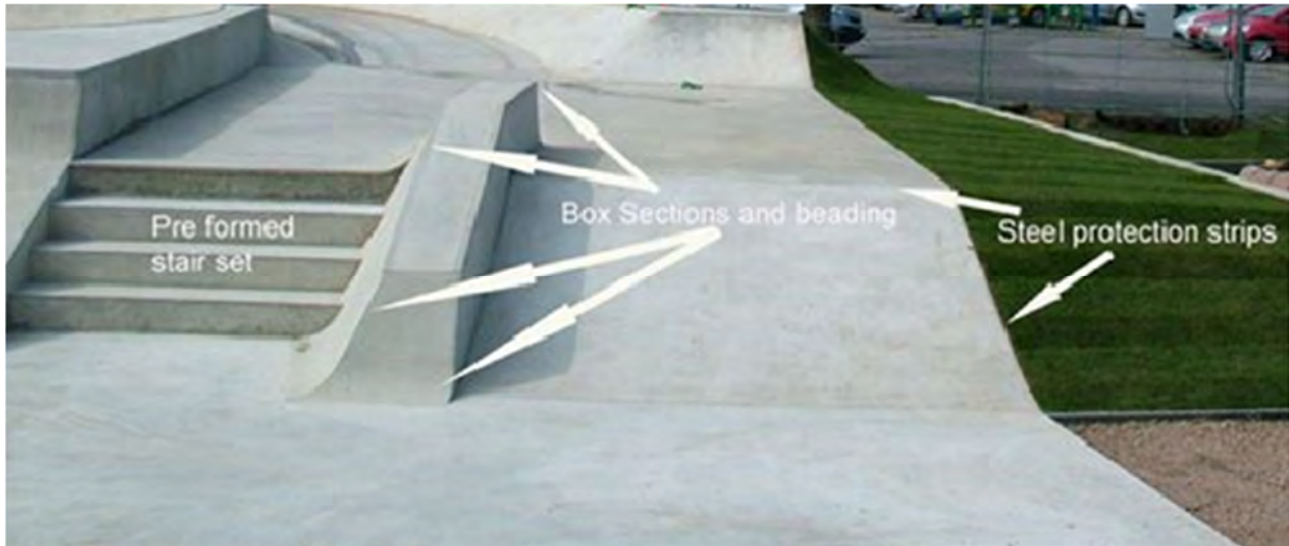
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Further examples of steel use can be seen in this picture of our skate park in Dorchester whilst clearly still in the build process. By no means exhaustive in terms of all the steels that could have been notated in just this picture, it does however give an indication as to why our parks hold up so well and are recognised as being technically perfect by the riders themselves



References

The following list notates the Codes and standards that were referenced partially or wholly in the above. The list is by no means exhaustive but covers every aspect of provision Maverick consider to be important.

EN 196-3 Methods of testing cement - Part 3: Determination of setting time and soundness EN 196-6 Methods of testing cement - Part 6: Determination of fineness

EN 197-1 Cement - Composition, specifications and conformity criteria - Part 1: definitions and compositions

EN 206 Concrete - Performance, production, placing and compliance criteria EN 480-1 Admixtures for Concrete, Mortar and Grout; Test Methods - Part 1: Reference concrete and reference mortar for testing

EN 480-2 Admixtures for Concrete, Mortar and Grout; Test Methods - Part 2: Determination of the setting time EN 480-6 Admixtures for Concrete, Mortar and Grout; Test Methods - Part 6: Infrared analysis

EN 480-8 Admixtures for Concrete, Mortar and Grout; Test Methods - Part 8: Determination of the conventional dry material content

EN 480-10 Admixtures for Concrete, Mortar and Grout; Test Methods - Part 10: Determination of water soluble chloride content

EN 934-2 Admixtures for Concrete, Mortar and Grout - Part 2: Concrete Admixtures - Definitions, Specifications and Conformity Criteria

EN 934-6 Admixtures for Concrete, Mortar and Grout - Part: Sampling, quality control and evaluation of conformity

EN 1008 Mixing Water for Concrete - Specification and Tests

EN 1542 Products and Systems for the Protection and Repair of Concrete Structures; Test Methods - Pull off test

EN 4012 Testing concrete - Determination of compressive strength of test specimens EN 4109 Testing concrete

- Determination of consistency - Slump test

EN 7034 Testing concrete - Cored specimens - Taking, examining and testing in compression



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Acknowledgements

In the production of this method statement some factual and technical information was drawn from many sources and due deference specifically to the BSI and EFNARC authorities and their websites has been paid and is acknowledged.

Reviewed 20th April 2017

Mark Clogg

Maverick Industries Ltd



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Llwynderw Skatepark – Design and Build

Management Procedures – A statement

To include Health and Safety, Quality Assurance, Risk Management

Health and Safety:

We pride ourselves on the professional approach to Health and Safety we have adopted since our inception. Naturally we are CHAS and Constructionline Accredited but we do not just pay lip service to H&S matters. We have a very well embedded culture of Safe Systems of Work and practices throughout our organisation which are constantly monitored and developed at Director level.

We operate an open door policy with our workforce and if they are able to identify methods of working that can improve site safety, these revised methods will be instantly incorporated into the appropriate method statements. We have a set of generic Risk Assessments that remain the same irrespective of the site we are working on, but we will also produce site specific Risk Assessments where needed along with a Pre-construction Health and Safety Plan and a Site Specific Construction Method Statement.

We are very proactive in keeping site crew informed of current legislation or indeed if the H&SE issue directives for employers to focus on issues such as (recently) airborne silica particulates being released into the atmosphere during the construction process and how to safeguard the site crews' wellbeing. We in such circumstances, meet with all the operatives and explain the notice that has reached our attention and the dangers it may represent. We then jointly formulate a way to control the risk to an acceptable level that will meet the H&SE's directives. In the case of the Silica notice, upgraded fully fitting face masks were provided and when concrete needed to be cut using a power saw, we instigated a regime where crews worked in pairs, one cutting and the other constantly spraying the saw blade with water to reduce the creation of dust to practically zero.

Each project we start begins with Maverick's H&S Officer – who is IOSH accredited - meeting the crew on site and holding a tool box talk where any site specific H&S issues are specifically highlighted. This is generally the cumulation of data gathered in desktop surveys, formal site surveys and locally sourced knowledge of historical works that may not show up on the various surveys carried out.

The most common issue we encounter is buried services, at our planning stage we will have ensured all works would meet with respective easement areas but any detected or known services will as a matter of course, be marked to ensure a visual representation of their existence remains for the duration of the works to ensure they are not accidentally disturbed.

We had previously alluded to our day to day H&S practices but in addition, we hold on a monthly retainer the services of MSAFE Ltd. to offer specialist advice whenever it is needed, and we hold an annual meeting with Nathan Pitt in our offices to run through our procedures and to cross check that we have identified all relevant H&S updates to ensure we are remaining compliant in all areas of our work.

Their website can be interrogated by pasting this link into your browser:

www.msafeco.uk



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REG. OFFICE
UNIT G2 . ARENA BUSINESS CENTRE
HOLYROOD CLOSE . POOLE . BH177FJ

WWW.MAVERICKINDUSTRIES.CO.UK
INFO@MAVERICKINDUSTRIES.CO.UK
TEL 01202 607 475 | FAX 08456529991

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Quality Assurance:

We believe we are alone in the UK skate industry in operating a Total Quality Management System known as Six Sigma. We have found this aligns itself with our requirements much more than the conventional ISO 9001 QMS.

Our superlative reputation for quality throughout the entire delivery process is set and recognised by both users and clients and the finished product speaks for itself. We therefore are confident in stating our TQMS is both appropriate to our needs and clearly works.

We have completed a draft set of Six Sigma documents which will obviously need development as time goes on but they will form the backbone of our TQMS which will run alongside all our future dealings.

Risk Management:

This heading can be interpreted in two ways. We believe we have already covered the risk management on site above under the Health & Safety heading, however we can also consider Risk Management as being the process of identifying, assessing and controlling an organisation's capital and earnings. Such threats or risks could stem from a wide variety of sources including financial uncertainty, legal liabilities, strategic management errors, accidents and natural disasters.

In short, Risk Management at Maverick refers to the practice of identifying potential risks in advance, analysing them and taking precautionary steps in advance to reduce or preferably avoid the risk entirely. When you interrogate our Six Sigma document, you will see that we place great store in ensuring any design we offer has been carefully costed. Every external financial pressure is considered and accounted for within our own costing tool. This therefore highlights at a very early stage if the design is financially viable and once set, allows us to project budgetary requirements. If we are at a stage where we have a formal order the scheme is then placed in our cash flow forecast which also highlights payment dates to the various HMRC departments and suppliers.

After eleven years of providing skate parks to the public sector we have become adept at identifying the projects we are most likely to win which helps us to understand in the broadest of terms, what our requirements may be up to eighteen months in advance. Likewise we have already won a great many projects which are not yet fully funded or perhaps have issues such as planning and the suchlike yet to overcome. All these factors feed into our overall "Big picture" and updates on these are usually gained on a monthly basis.

Our accountant spends an afternoon each week with us updating and analysing cash projections as well as tracking our invoices to ensure we have a very robust understanding of our weekly financial wellbeing for up to three months in advance.

We have always believed in paying our suppliers on time as well as honouring our obligations to the crown in a timely fashion which ensures we do not enter the realms of long term historic debt. Keeping a finger on our fiscal pulse is common sense and is a professional and sensible way to run our business. We know well in advance when large payments will need to be made and by judicious application of cost control, we can ensure our obligations are met without compromising our ongoing cash flow forecasts.

Fortunately legal liabilities can be kept to a very manageable and minimal amount by simply running the company ethically and honestly. Strategic management errors are also kept in check by the way we operate. No single decision of strategic importance is made by just one person. Every decision that can have a bearing on the company's direction is discussed and analysed by all the directors together and sometimes with our accountant as well if it is seen to be desirous.

We keep returning to the Six Sigma TQMS. By understanding our business at a micro level we are able provide the client with a proposal that will meet their aspirations and will conform to their definition of success. We know we will be able to deliver it for the fixed sum we have promised and equally importantly, we have the financial stability built in from the start that will enable us to fulfil our obligations in the long term to the client should any remedial work be required during the guarantee period.



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Accidents in the real world cannot be entirely ruled out. All we can do is to put in place systems of work that are proven to be safe, provide a working environment that is conducive to SSOW and give crews the right tools to do their job. *Should* something unforeseen happen, we have the policies, procedures and back up suppliers in place that will allow us to mitigate the effect of such an event to the degree that the delivery team or supply chain will appear to be unaffected.

Keeping site equipment in good order is part of site crews daily routine. Equipment failure at the wrong time can have knock on consequences affecting programming as well as financials.

We took the decision several years ago to purchase our own concrete pump which ensures the most important piece of equipment we use is entirely under our own control. We know it to be fit for purpose, clean, well maintained and only ever used by ourselves.

Perhaps using the term natural disasters may sound like over kill but one thing we have no control over is the weather. Unseasonal weather trends can catch anyone out, but our site build programming has the ability to be flexible should prevailing conditions temporarily preclude certain procedures. Likewise, we can take a pragmatic view of what we are likely to achieve during a given period of the year and a projected build programme will have considered and costed for, anticipated seasonal weather patterns. Each project also has a contingency period built in to allow for the theoretical “unseen eventualities”.

It is this level of understanding and preparedness that has allowed us to become the success we are.



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Minutes of Council Meeting

9 June 2020

MINUTES of the MONTHLY MEETING of MUMBLES COMMUNITY COUNCIL held via Zoom on Tuesday 9 June 2020

Present:

113.06	Councillors(s)	Councillors(s)
.2020	Pam Erasmus	Martin O'Neil
	Rebecca Fogarty	Phillip Reason
	Gareth Ford	Ian Scott
	Adam Gilbert	Carwyn Thomas
	Richard Jarvis	Will Thomas
	Myles Langstone	Carrie Townsend Jones
	Sara Keeton	Linda Tyler-Lloyd
	Rob Marshall	

114.06 In attendance:

Steve Heydon (Clerk), Paul Beynon (RFO)

115.06 Apologies for Absence

Councillors: Tim Bull & Rebecca Singh

116.06 Declarations of Interest

Rebecca Fogarty declared an interest in item 166.06 as her husband runs a similar company to the Lighthouse Theatre

Cllr Myles Langstone declared a personal interest in item 170.06 as he objected to the original proposal

Rob Marshall declared an interest in item 166.06 as he is friends with the Lighthouse Theatre.

Cllr Ian Scott declared a personal interest in item 170.06 as he objected to the original proposal.

Cllr Will Thomas declared a personal interest in item 170.06 as he objected to the original proposal.

117.06 Minutes of the Monthly Meeting held on 10 March 2020

RESOLVED to confirm as a correct record the minutes of the Monthly Meeting held on 10 March 2020.

118.06 Minutes of the Special Meeting held on 11 May 2020

RESOLVED to confirm as a correct record the minutes of the Special Meeting held on 11 May 2020.

119.06 Minutes of the Annual Meeting held on 19 May 2020

RESOLVED to confirm as a correct record the minutes of the Annual Meeting held on 19 May 2020.

120.06 Minutes of the Special Meeting held on 27 May 2020

RESOLVED to confirm as a correct record the minutes of the Monthly Meeting held on 27 May 2020.

121.06 Clerk' Report

The Clerk's Report was circulated with the agenda pack.

RESOLVED that this be noted.

122.06 Chair's Report

The Chair's Report was circulated with the agenda pack.

RESOLVED that this be noted.

Finance Report

Electronic Payments for Approval			£	£	£
123.06	23/03/20	Barclaycard March 2020 Statement – see below	1,033.26	0.00	1,033.26
124.06	21/04/20	Barclaycard April 2020 Statement – see below	539.34	0.00	539.34
125.06	29/04/20	Alain Thomas Consultancy – Advance of Fees	350.00	0.00	350.00
126.06	01/05/20	Ostreme Community Association - Rent	800.00	0.00	800.00

127.06	04/05/20	Alain Thomas Consultancy – Evaluation March 2020	956.00	0.00	956.00
128.06	04/05/20	Peter Lynn & Partners - Ostreme	1,653.50	330.70	1,984.20
129.06	04/05/20	Peter Lynn & Partners - Ostreme	393.90	76.38	470.28
130.06	04/05/20	Peter Lynn & Partners – Ostreme	261.30	52.26	313.56
131.06	15/05/20	Barracwda – Dragon Parade	600.00	0.00	600.00
132.06	15/05/20	Picseli – Multimedia Consultant May 2020	416.67	83.33	500.00
133.06	15/05/20	Complete Self Storage – May 2020	116.67	23.33	140.00
134.06	15/05/20	Ffion Powell – Volunteer Shopping Reimbursement	68.27	0.00	68.27
135.06	15/05/20	Sally Webber – Volunteer Shopping Reimbursement	22.75	0.00	22.75
136.06	20/05/20	Upper Norton Field Allotment Association - Grant	500.00	0.00	500.00
137.06	20/05/20	Amanda Evans – Volunteer Shopping Reimbursement	35.21	0.00	35.21
138.06	21/05/20	Barclaycard May 2020 Statement – see below	640.97	0.00	640.97
139.06	22/05/20	Virgin Media – Broadband/Phone May 2020	73.93	14.79	88.72
140.06	22/05/20	Cllr Richard Jarvis – Councillor Allowance 2018/19 and 2019/20	130.10	0.00	130.10
141.06	22/05/20	Ffion Powell – Volunteer Shopping Reimbursement	40.99	0.00	40.99
142.06	25/05/20	Staff Salaries - May 2020	2,534.11	0.00	2,534.11
143.06	26/05/20	Sally Webber – Volunteer Shopping Reimbursement	40.16	0.00	40.16
144.06	29/05/20	Gareth Bowen – Volunteer Travel Expenses	28.35	0.00	28.35
145.06	29/05/20	HMRC – Payroll Deductions Month 2	185.46	0.00	185.46
146.06	01/06/20	Sally Webber – Volunteer Shopping Reimbursement	22.50	0.00	22.50
147.06	01/06/20	Heatwave Marketing – COVID-19 Co-ordinator	726.23	0.00	726.23

148.06	01/06/20	Ostreme Community Association - Rent	800.00	0.00	800.00
150.06	02/06/20	Mumbles Community Association – Professional Fees	16,298.00	0.00	16,298.00
151.06					
152.06		Total	29,267.67	580.79	29,848.46
154.06	Barclaycard Purchases – March 2020 Statement				£
		Apple – iPad Additional Storage			0.79
		Catalyst2 Services – Website Hosting			15.59
		Reach Publishing – EEO Advert			480.00
		Survey Monkey – Annual Subscription			408.00
		Giff Gaff – RFO Mobile Phone			6.00
		Microsoft – Online Services			100.32
		Microsoft – Online Services			22.56
		Total			1,033.26
155.06	Barclaycard Purchases – April 2020 Statement				£
		Apple – iPad Additional Storage			0.79
		Catalyst2 Services – Annual Licence			118.80
		Catalyst2 Services – Website Hosting			15.59
		Giff Gaff – RFO Mobile Phone			6.00
		Fluid Branding – Bamboo Pens			249.60
		Microsoft – Online Services			100.32
		Microsoft – Online Services			48.24
					539.34

156.06	Barclaycard Purchases – May 2020 Statement	£
	Apple – iPad Additional Storage	0.79
	Vodafone – Mobile Phone Top Up	10.00
	Amazon – RFO Printer Cartridges	35.70
	Catalyst2 Services – Website Hosting	15.59
	Giff Gaff – RFO Mobile Phone	6.00
	Microsoft – Online Services	100.32
	Microsoft – Online Services	45.12
	EE – Mobile Phone Top Up	10.00
	Zoom – Monthly Fee	14.39
	Kingsbridge Print – COVID Leaflets/ID Badges/Lanyards	339.12
	Argos – EEO Computer Storage	63.94
	Total	640.97
157.06	Income	£
	01/06/20 NEST – Pension Contribution Refund	117.03
	Total	117.03
158.06	Bank Balances at 04/02/2020	£
	Current Account	29,935.76
	Savings Account	645,594.47
	Total	675,530.23
159.06	Cllr Reason joined the meeting.	
160.06	The meeting was adjourned to allow Alain Thomas to present his report.	
161.06	A Review of the Council's structures, processes, and staffing arrangements	
	Alain Thomas introduced his report to councillors.	

162.06 Myles Langstone left the meeting

163.06 Skate Park Funding Proposal

PROPOSED by Rebecca Fogarty

SECONDED by Pam Erasmus

PROPOSE that, following the advice of the RFO and the Clerk, the attached funding package for the skatepark be put into place to satisfy the conditions for proven sustainable funding for the Mumbles Skatepark, demanded by the due diligence exercise of Swansea Council.

- a) That Mumbles Community Council underwrite any shortfall in funding up to a total of £362,240 in 2020-2021 to enable the Skatepark to be built in this financial year
- b) That a stage 1 grant application be submitted to Community Lottery Fund Wales.

A named vote was called:

For: Cllrs: Pam Erasmus, Rebecca Fogarty, Gareth Ford, Adam Gilbert
Richard Jarvis, Sara Keeton, Rob Marshall Martin O'Neil, Phillip Reason
Ian Scott, Carwyn Thomas, Will Thomas, Carrie Townsend Jones

Against: None

Abstain: Cllr Linda Tyler Lloyd

RESOLVED that the motion be PASSED

Items from the Cultural Well-being Committee

164.06 Name of Committee

As many of the events and activities organised by the Committee are aimed at tourists and visitors as well as the local community.

RESOLVED that the name of the Committee be changed to the Cultural and Tourism Well-Being Committee.

165.06 Re-establishment of Mumbles Chamber of Trade

The committee believe that it would be beneficial if the Mumbles Chamber of Trade was re-established to help support shops and businesses going forward. The intention is to include all commercial areas of Mumbles and

ideas included improved signposting and production of a map to guide visitors. It is also felt that the Mumbles Chamber of Trade could help to attract sponsors for future events being arranged by the Committee.

RESOLVED that the Council supports the re-establishment of the Mumbles Chamber of Trade and a working group be set-up to plan the re-establishment of the Chamber of Trade.

166.06 Mumbles Guided Walks

The Committee budget includes £1,500 for the Lighthouse Theatre which it is proposed is used to develop, research, create and write four filmed walking tours of Mumbles of roughly 15 minutes duration. The videos will be displayed on media portals and websites associated with Mumbles, Swansea Bay and the Lighthouse Theatre.

RESOLVED that a payment of £1,500 is made to the Lighthouse Theatre to create four filmed walking tours of Mumbles from the Mumbles Guided Walks budget.

167.06 Myles Langstone re-joined the meeting

Recommendations by the Community & Social Well-being Committee

168.06 Underhill – MCA/MCC Group

RECOMMEND that Martin continue to Chair the group and MCC representatives be Chair of Community & Social Well-being Committee, the Vice Chair of Council (as Chair could not fill the role) be appointed, together with one other councillor appointed by Council.

Two candidates – Carwyn Thomas & Carrie Townsend Jones were proposed and seconded.

A named vote was called:

For Carwyn Thomas: Cllrs: Adam Gilbert, Myles Langstone, Rob Marshall, Martin O'Neil, Phillip Reason, Ian Scott, Carwyn Thomas, Will Thomas & Linda Tyler-Lloyd

For Carrie Townsend Jones Cllrs: Pam Erasmus, Rebecca Fogarty, Gareth Ford, Richard Jarvis, Sara Keeton & Carrie Townsend Jones

RESOLVED that Cllr Carwyn Thomas be appointed.

Recommendations by the Environmental Well-being Committee

169.06 Co-opting & Appointing Members of the Committee

RESOLVED that Ruth Cronin & Ed Hall be co-opted members of the committee

Recommendations by the Planning Sub-committee

170.06 Planning Application 2018/2634/FUL - 31 Dwellings on Land off Higher Lane

RESOLVED that Mumbles Community Council objects to this planning application on the grounds of the issues raised in the Lichfield's report commissioned by the Council which have not been addressed by the developer and the Statement of Common Concerns and Expectations produced by the community in response to the latest consultation on the application.

171.06 Appointment of Committees

RESOLVED that:

Cllr Pam Erasmus be appointed to the Environmental Well-being Committee

Cllr Sara Keeton & Richard Jarvis be appointed to the Cultural & Tourism Well-being Committee

(and after the following named vote:

For Ian Scott: Cllrs: Myles Langstone, Phillip Reason, Ian Scott, Carwyn Thomas, Will Thomas & Linda Tyler-Lloyd

For Sara Keeton Cllrs: Pam Erasmus, Rebecca Fogarty, Gareth Ford, Martin O'Neill, Richard Jarvis, Sara Keeton & Carrie Townsend Jones)

Cllr Sara Keeton was appointed to the Community & Social Well-being Committee

Meeting closed at 8:20 pm



Skatepark Funding Proposal

PROPOSE that, following the advice of the RFO and the Clerk, the attached funding package for the skatepark be put into place to satisfy the conditions for proven sustainable funding for the Mumbles Skatepark, demanded by the due diligence exercise of Swansea Council.

- a) That Mumbles Community Council underwrite any shortfall in funding up to a total of £362,240 in 2020-2021 to enable the Skatepark to be built in this financial year
- b) That a stage 1 grant application be submitted to Community Lottery Fund Wales

Introduction

The Mumbles Skatepark is one of the most supported developments that Mumbles Community Council has ever proposed. This proposal is put forward following the resolution that was approved at the Special Meeting on May 27th: 'that our Council supports the proposal, which enables progress on the facilities at Underhill Park without jeopardising the ability of MCC to progress other projects to which it is committed in this financial year'. The proposed funding package outlined here clearly demonstrates how this can be achieved with regards to one of those projects, namely Mumbles Skatepark.

The expectation of grant funding for the Skatepark has now significantly changed, meaning that while grants are potentially available, it cannot be considered as guaranteed funding. In order to have the Skatepark operational by next Summer, the business plan demonstrating proven sustainable funding will need to be submitted to Swansea Council as soon as the independent consultant has completed their work (estimated end of June 2020), in order for Cabinet to agree the lease and for work on the site to start.

Funding

The total funding required to complete the build and landscaping of the Skatepark is £362,240. Annual revenue costs will be a maximum of £12,689 and most likely considerably less.

CAPITAL Costs:

Build plus:

Landscaping	
fencing and hedging	
wheelchair accessible picnic benches and tables	
bike racks x2	
planters	
palm trees	
gates	
litter bins	£334,990
Water Fountain supply and installation, including laying water supply	£7,500
Additional Benches and Bins	£2,500
Contingency (5%)	£17,250
TOTAL	£362,240

REVENUE Costs (per annum):

Grass Cutting and Bin emptying (Swansea Council contract)	£3,715
Annual Inspections (RoSPa Play Safety Ltd)	£400
Cleaning/painting/general maintenance	£1,500
Graffiti removal (allows for 10 incidents per year)	£150
Insurance (allows for unlimited Skateparks, Zip wires and BMX tracks)	£224
Sinking Fund representing 2% of build cost (expected minimum 25 year life)*	£6,700
TOTAL	£12,689

*The Sinking Fund represents the ringfencing of a set amount from the precept each year to build up a fund to pay for major repairs and refurbishment as they arise during the lifetime of the facility. This is something that all capital projects should allow for and is a specific requirement of Swansea Council.

This proposal sets out how these costs can be covered.

CAPITAL Costs:

The National Lottery Community Fund Wales, along with all other Lottery Funds, are currently prioritising applications that relate to the Coronavirus pandemic:

We are prioritising applications from:

1. Organisations supporting people who are at high risk from COVID-19

2. Organisations supporting communities most likely to face increased demand and challenges as a direct result of COVID-19
3. Organisations with high potential to support communities with the direct and indirect impact of COVID-19

(Source: <https://www.tnlcommunityfund.org.uk/funding/wales> - accessed 03/06/2020)

This does not mean MCC cannot apply, but it does mean that our application will, to all intents and purposes, be put on hold until the crisis is over. The National Lottery Community Fund Wales cannot give a guarantee of when our application would be processed and it may not be prioritised at all if the health and/or economic situation worsens or continues beyond October.

Therefore the following proposal will enable the Skatepark to be built this year without itself jeopardising any other projects if, as expected, grant funding is not available:

Source	Amount
Earmarked Funds	£130,00
Previously identified underspends from 2019-20 and virement of funding not otherwise needed in this financial year (2020-21)	£232,240

EXPLANATORY NOTES

- a) £130,000 is already earmarked in this year's budget for the Skatepark.
- b) The Underhill Park proposal identified £360,000 of underspends and potential virements. Reallocating £232,240 to the Skatepark will not affect the funding required for Underhill in this financial year. It will leave £127,760 available to complete the refurbishment of the Underhill Pavilion (costing £100,000), providing the first of the new changing rooms and the much-needed public toilets in this financial year. This will ensure that both these flagship projects can get underway this year – as well as completion of the first two Tennis Courts at Langland.
- c) The current lockdown has caused the cancellation of planned events and activity which can no longer go ahead. There are therefore likely to be additional underspends to cover unexpected demands within this financial year.
- d) This proposal will not jeopardise the main building project for Underhill, as loan funding is being requested, and is therefore outside the scope of MCC current funds.
- e) The funding towards the all-weather pitch is not required until at least Year 3 of the Underhill project. This can be recouped over the next two or possibly three years through the precept.
- f) Ongoing revenue costs are minimal. Annual maintenance costs can be more than covered without recourse to MCC funding (detail provided below). The only draw on MCC resources is the Sinking Fund annual reserve of £6,700, as described above.
- g) We have already been approached by one major business wishing to sponsor the Skatepark. Cllr Reason is confident there will be more wishing to do so.

- h) Mumbles Skatepark Association is a Charitable Association set up to raise funds towards the ongoing maintenance, and to add extra facilities to, the new Skatepark. It is already actively fundraising.

REVENUE Funding (per annum)

Our aim is wherever possible for the Skatepark to be 'self supporting' for costs and maintenance. MCC have already been approached by a local business interested in sponsoring the park. No figure has yet been agreed.

Local annual business sponsorship will include advertising opportunities on the park, social media and internet promotions linked to the park and naming rights for some events held at the park.

All amounts are estimates at present but we feel confident these amounts can be achieved.

Sponsorship

Main sponsor	12 x payments of £350	£4200
Secondary sponsors (smaller one off support from local businesses)		£1500

Revenue from events (organised by the skaters)

Annual national event for x 100 participants at £5 each	£500
3 smaller local events x 50 participants at £3 each x 3 events	£450
Mumbles Skatepark Association funding	£1,000
Estimated total – revenue funding	£7,650

Revenue from grant funding will also be explored.

Conclusion

We all appreciate that the pandemic has had a truly terrible impact on all our lives. The lockdown has restricted our ability to interact with friends and family in the beautiful setting that we are so lucky to call home, with many experiencing a negative impact on their physical, mental and social well-being. However, in Mumbles we have witnessed the strength of our community as we have come together to battle the impacts of the Coronavirus crisis. We believe that MCC is in a unique position to celebrate our community, and its determination, by providing the community the facilities that will reward and sustain this resilience. Built facilities are central in encouraging community members to interact and enjoy the environment they live in. This proposal demonstrates how we can have not one, or two, but three fantastic community facilities within this financial year.

Langland Tennis Courts – win

Underhill Park – win, win

Mumbles Skatepark – win, win, win!

Thank You!



Maintenance & Guarantee Statement

Llwynderw Skatepark

By choosing a skate facility made from in situ sprayed concrete the usual issues concerned with Maintenance and Durability can largely be set aside. Our superior method of finish will ensure your maintenance will be restricted to a series of simple visual checks to ensure undesirable foreign objects such as broken glass, sharps or even makeshift wooden obstacles have not found their way onto the park.

In Autumn months any leaf fall in the park should be removed as when they become wet they will present a real slip hazard to the users. We suggest a broom could be made available to the users so they can clear the park themselves as and when is needed in addition to a scheduled council operative clean.

We suggest a simple sign off book register is employed to state the date and time inspections are carried out, by whom and if anything untoward was found, and what arrangements have been made to rectify whatever fault has been found.

Since we are specifying a spray concrete facility there are NO fixings to break or come loose, there will be no safety hand rails that can fail. In reality all that is likely to be needed to be done is to litter pick the park and empty the bins. The park will not need to be treated with any specialist products.

The council will need to decide on their stance re graffiti. We can recommend products supplied by others to remove this. Generally, our parks for whatever reason do not appear to attract tagging on the riding surface. This is more about enforcing the local viewpoint and engendering respect amongst locals.

We recommend 'Graffiti Go' as a suitable product for removing Graffiti. Applied with care, and in line with the instructions then sponged off, this will do the job. You can purchase this directly on the following link:

<https://www.toolstation.com/shop/p21232?table=no>

Jet washing should only be used as a last resort as this can remove the cream from the concrete if not carried out with great care.

It should be noted that ALL concrete parks will at some point develop hairline cracks. With the reinforcement sheeting and steel "spiders web" arrangements in place and unseen on the finished article, this is absolutely of no consequence whatsoever. It is even to be expected. The design of the park from an engineering platform ensures it will be stable and will conform to all the relevant guide lines; however, you will have a full set of engineering drawings that will confirm that your park has been designed correctly.

In the highly unlikely event of finding a structural fault with one of our parks we simply ask you make contact with our office and send an email with pictures of what is concerning you. We will take whatever action is appropriate upon receipt.

The high C values that we build our parks to will ensure our facility is durable and completely fit for purpose. Anywhere where we would anticipate the users will "grind" has been protected by galvanised box steel, this will stop the chipping one usually sees in inferior park designs.



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Any edges such as floors or platforms which would not be used in such a way have been treated with a “rounding tool” this creates a radiused edge which not only adds an aesthetically pleasing finish, it will also deter cracks or chipping from appearing. The details of this will be found in the supplied engineering drawings and method statements.

We are aware that BMX bikes with “stunt pegs” have been known to damage skate parks, some scooters also have metal pegs. It is completely acceptable to state that metal pegs are NOT ALLOWED on this facility. There are many such rules in force in skate parks across the country and they tend to be self policed and enforced by the users themselves. The low decks on scooters can cause scuff marks on the concrete which though unsightly, is an unavoidable part of the wear and tear that a well used facility will experience.

We have done everything in our power to protect the park by ensuring every grindable edge is covered by the galvanised box section.

We are confident that our specification will stand the test of time as we have projects in the ground that are not showing adverse wear and tear after many years of use.

The beauty of providing a facility in spray concrete is the ease in which it can be repaired if it is ever needed. Any person with basic home DIY skills can mend any slight defects, we would advise that you simply contact us with a picture of what is worrying you, we will advise the appropriate course of action and if anything is required, it is likely that you will only need a small amount of one of these two (or similar) products:

<https://www.allfix.co.uk/productgrp/spit-c-mix-plus-polyester-resin-basic>

<https://flexcrete.com/flexcrete-products/concrete-repair-products/concrete-repair-mortars/monomix-hd-concrete-repair/>

Copings at weld points can be maintained if necessary by utilising Anti-Rust Galv Spray:

<https://www.actioncan.com/product/zg-90/>

Other areas where welds are visible should be checked for integrity and can likewise be treated with Galv Spray should “grinding” wear off the protective zinc we applied when the park was built.

If a formalised drainage system has been specified with this project and we will provide you with a Hex key in order for the drain to be checked and cleaned. The sump pot under the drainage cover may need occasional cleaning of debris to ensure the drains remain free and clear. Twice a year is generally sufficient. We suggest that the screw threads are dipped in grease before winding in place to ensure they will be able to be removed easily on the next inspection date. Where Aco drains are specified, covers need to be lifted and cleared of debris on a regular basis.

In line with standard JCT contract terms, we allow our clients to hold a retention during the 12 months Defects Liability Period (DLP) and in addition we will offer a 15 year guarantee against structural failure. The level of detail in engineering we go to ensures your park will be constructed to the highest possible standards.

At the end of the defects liability period we will return to site to carry out any necessary snagging.

Please note: Park defects and erosion of bunds caused by wear and tear, arson, deliberate misuse or wilful neglect are not covered by the guarantee, nor are any remedial works to landscaping works including trees.



Any ancillary items supplied such as shelters, benches & bins are also excluded from our guarantee but are covered by the standard warranty provided by the manufacturer.

It should be noted that Pool Copings & tiles whilst popular with the riders are prone to chipping when pegs are used in the park. For this reason regrettably they are excluded from our guarantee.

Upon practical completion and handover of the facility, responsibility for maintaining the facility and the surrounds, including watering until the bunds have grassed over and tree care passes to the client.

The quality of all the existing parks we have put in is testament to their predicted durability and projected long lifespan. We guarantee you will not be able to source a finer built skate park than one built by Maverick.

Data Sheets

Data sheets for these materials will be provided within our 'As Built' information at the end of the project.

On-going Maintenance Cost Summary

It is difficult for us to precisely quantify costs this as we are not party to the wage structure your operatives enjoy. If you are able to incorporate the suggested visual checks of the skatepark into existing arrangements for other facilities, we would suggest that cost is already budgeted for and as a consequence will not have any significant additional financial impact.

Cleaning of the park will often be undertaken by the users themselves if they have the ability to do so, and this should be encouraged - however you may wish to factor in during Autumn months a regime to clear the park of leaves and with bins being filled regularly and therefore needing emptying, whatever cost that entails for the council will need to be factored in.

There is absolutely no need for an additional annual paid for maintenance plan.

Should minor cosmetic repairs be required during its life cycle we have provided you with links showing costs to buy the products (at today's prices). You would only have to apportion an hourly rate to which ever member of your ground staff you would task with dealing with it. However, we would suggest that most repairs can be effected in under an hour based upon historical evidence.

Providing the park is looked after well, those costs should be all you will need to budget for during the park's lifetime.

Mark Clogg

Maverick Industries - Health and Safety Officer

01.11.18



MAVERICK
I N D U S T R I E S

REG. OFFICE
UNIT G2 . ARENA BUSINESS CENTRE
HOLYROOD CLOSE . POOLE . BH177FJ

WWW.MAVERICKINDUSTRIES.CO.UK
INFO@MAVERICKINDUSTRIES.CO.UK
TEL 01202 607 475 | FAX 08456529991

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Mumbles Skatepark Association - Fund raising plans

The aim of the fundraising will be to support added value items to the site, such as but not limited to:

- additional seating
- water fountain
- additional bike racks
- advertising and infrastructure for events

What	Description and Progress to Date	Estimated income
Merchandise T-shirts, sweatshirts and stickers	We plan to work with local artists connected with the Art shows/Gallery events mentioned below. We already have suppliers/printers sourced and have also explored a pre order facility in the first instance (where you encourage pre orders to ensure you meet the minimum print number and, once met, the orders are shipped directly to individuals. As we build up funding, we can then set a print t run to sell at all events listed below.	Based on previous experience selling t-shirts etc to support bands and events, we anticipate a 100%/£10 profit on a one colour print design, with an initial run of 50 shirts.
Gigs	We aim to do gigs based in town and in Mumbles to support fundraising. We have spoken to bands and have identified a venue. We have also liaised with local breweries to support re bar on a sale or return basis	Based on previous fundraiser events members of the association have been involved with, we anticipate somewhere in the region of £1000 raised per gig on entrance price and bar takings. This would also be supplemented by raffles and merchandise sales
Skate quiz night	We have prepared several quizzes spanning skateboarding history from the 1960s to the present day and have run “online pilots” with a cross section of the community. We have also had initial discussions with a local venue in terms of running a monthly or quarterly event (dependent on initial uptake)	We anticipate somewhere in the region of £200 raised per quiz night This would also be supplemented by raffles and merchandise sales
Sponsored beach clean	This will be dependent on involvement of local schools and other organisation/associations. The main focus of this event will be community engagement and a shared ownership of the site and its surrounding area. This will also help to engage the community with a shared sense of ownership moving forward, aimed to reduce litter, graffiti etc organically	Dependent on uptake, based on previous sponsored skate events, we anticipate somewhere in the region of £500 to be raised

Art shows/Gallery events	<p>We have spoken to local artists and Are awaiting responses from known international skate related artists. We have identified potential venues and have also liaised with local breweries to support re bar on a sale or return basis</p> <p>We would anticipate running annually, dependent on the success of the event</p>	<p>Based on previous fundraiser events members of the association have been involved with, we anticipate somewhere in the region of £1000 raised per event.</p> <p>This would also be supplemented by raffles and merchandise sales</p>
Cinema and Co sponsored film events	<p>Cinema and co have already offered to run and outdoor cinema in aid of the association, with the intention that £2 of the ticket price will go towards the association.</p> <p>In addition, they intend to involve local vendors who will also either Donate a fee to the association for participating or Donate a percentage of takings on the night</p> <p>Cinema and Co have indicated they would happily run this annually or more often, dependent on uptake</p>	<p>We anticipate somewhere in the region of £250 to be raised</p> <p>This would also be supplemented by raffles and merchandise sales</p>
Sponsorship	The Association has made initial approaches to local shops/businesses and national distributors in relation providing product/prizes	Prizes to support quizzes, contexts, raffles etc
Beer Riff Sponsored Beer	Beer Riff have agreed to develop, brew and sell a bespoke craft IPA in support of the skatepark and the association will benefit from a percentage of the sales	% of sales